

APPLICANT

SLAVIC CHURCH EMMANUEL  
18001 SE FOSTER RD  
DAMASCUS, OREGON 97089-5553

CIVIL ENGINEERING

CITY OF PORTLAND  
BUREAU OF TRANSPORTATION  
ENGINEERING SERVICES DIVISION  
1120 SW 5TH AVENUE, SUITE 800  
PORTLAND, OREGON 97204

SITE INFORMATION

ADDRESS: 8025 N.E. 82ND AVENUE  
SITE AREA: 3.50 ACRES  
TAXLOT: 100  
TAX MAP 1S 2E 05DD

SHEET INDEX

ST-01 COVER SHEET WITH SITE AND VICINTIY MAP AND LEGEND  
ST-02 GENERAL CONSTRUCTION NOTES  
ST-03 TYPICAL SECTIONS  
ST-04 PLAN AND PROFILE  
ST-05 ELEVATION DETAIL PLAN  
ST-06 SIGN LEGEND AND POST DATA SHEET  
ST-07 HORIZONTAL ALIGNMENT PLAN  
EC-01 EROSION CONTROL PLAN

STANDARD DRAWINGS

P-400 - SIGN PLACEMENT  
P-405 - BREAKAWAY TRAFFIC SIGN SUPPORTS  
P-410 - SIGN BRACKET, CAP DETAILS  
P-411 - SIGN BRACKET, STRONG-BACK DETAILS  
P-420 - STREET NAME SIGN DETAIL  
  
P-506 - ASPHALT CONCRETE PAVEMENT REPAIR ADJACENT TO CURB  
P-507 - MANHOLE FRAME ADJUSTMENT  
P-528 - TYPICAL SEPARATED SIDEWALK DRIVEWAY  
P-536 - DRIVEWAY CONNECTION DETAIL  
P-540 - CURBS  
  
P-547 - MIDBLOCK SIDEWALK RAMP  
P-548 - SIDEWALK RAMP WITH PLANTER STRIP PLACEMENT OPTIONS  
P-551 - SIDEWALKS  
P-581 - TYPICAL STREET TREE INSTALLATION

ODOT STANDARD DRAWINGS

RD759 - TRUNCATED DOME DETECTABLE WARNING SURFACE  
DETAILS & PLACEMENT LOCATION  
RD1010 - INLET PROTECTION  
TM800 - TABLES, ABRUPT EDGE AND PCMS DETAILS  
TM820 - TEMPORARY BARRICADES  
TM821 - TEMPORARY SIGN SUPPORTS  
TM840 - CLOSURE DETAILS  
TM841 - INTERSECTION WORK ZONE DETAILS

NOTICE TO EXCAVATORS:

ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER.  
RULES ARE ALSO LOCATED AT WWW.DIGSAFELYOREGON.COM  
NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION CENTER IS (503)-232-1987.

POTENTIAL UNDERGROUND FACILITY OWNERS

Dig Safely.

Website: <http://www.callbeforeyoudig.org/>  
Call the Oregon One-Call Center  
DIAL 811 or 1-800-332-2344

EMERGENCY TELEPHONE NUMBERS

NW NATURAL GAS  
M-F 7am-6pm 503-226-4211 Ext.4313  
AFTER HOURS 503-226-4211  
PGE 503-464-7777  
CENTURYLINK 1-800-573-1311  
BUREAU OF TRANS./MAINT. OPERATIONS 503-823-1700  
CITY WATER 503-823-4874  
VERIZON 1-800-483-1000

UTILITY CONTACT INFO:

CENTURY LINK COMMUNICATIONS:  
SCOTT MILLER  
PHONE 503-242-4144

PORTLAND GENERAL ELECTRIC 24-HOUR  
CONTACT:  
PHONE 503-464-7777

COMCAST:  
LEROY SOUMOKIL  
PHONE 503-596-3770

NW NATURAL:  
JODI WRIGHT  
PHONE 503-226-4211 X6745

CITY OF PORTLAND SEWERS:  
JOHN BUSHARD  
PHONE 503-823-6566

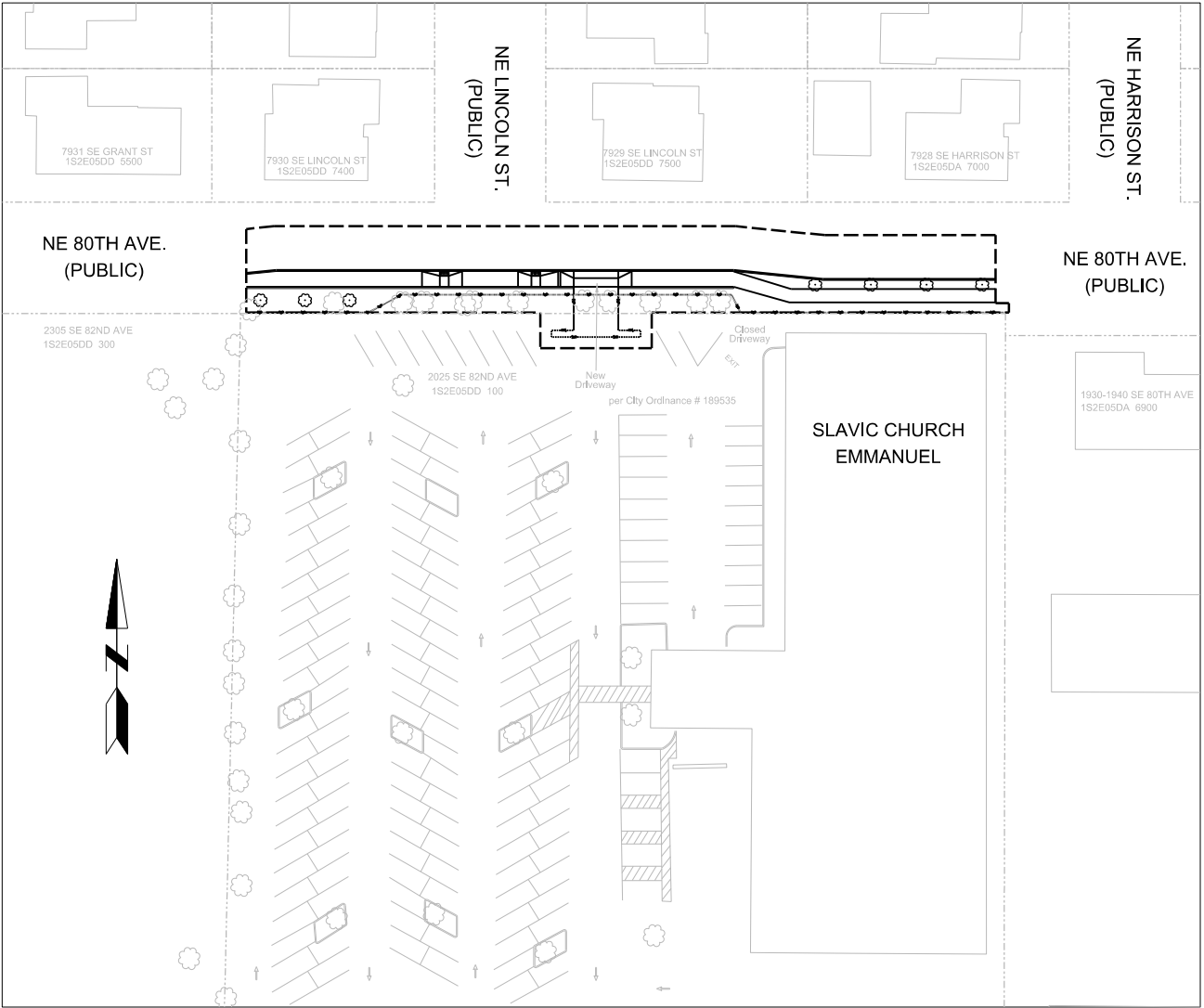
CITY OF PORTLAND WATER BUREAU:  
ROY MARTINEZ  
PHONE 503-823-8311

CITY OF PORTLAND SIGNALS/LIGHTING:  
DAN SPOELSTRA  
PHONE 503-823-4111

# PUBLIC STREET IMPROVEMENTS

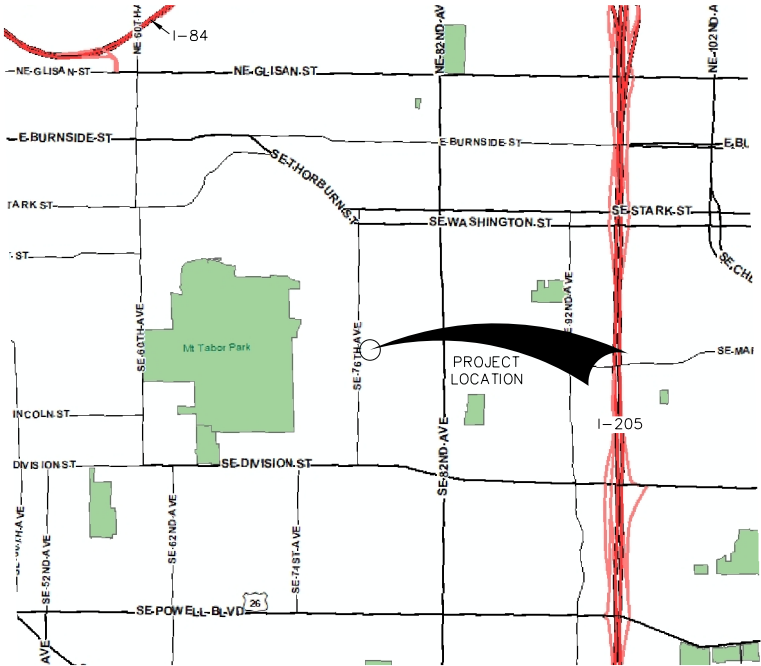
## 2025 S.E. 82nd AVENUE

### S.E. 80th Avenue Frontage



SITE MAP

SCALE 1" = 40'



VICINITY MAP

NOT TO SCALE

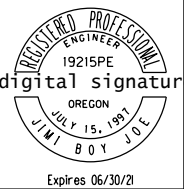
LEGEND

- |               |                               |   |                           |
|---------------|-------------------------------|---|---------------------------|
| .....TOP..... | Proposed Top of Slope         | ⊙ | Existing Sanitary Manhole |
| ---SAW---     | Proposed Sawcut               | ⊗ | Proposed Tree             |
| —EOP—         | Proposed Edge of Pavement     | ⊕ | Existing Water Meter      |
| -----         | Existing Right of Way         | ⊙ | Existing Tree             |
| —X—           | Existing Fence                | ⊙ | Existing Shrub            |
| —W—           | Existing Water Line           | ⊙ | Proposed Sign             |
| —G—           | Existing Gas Line             |   |                           |
| —EOG—         | Existing Edge of Gravel       |   |                           |
| -----         | Existing Edge of Pavement     |   |                           |
| ---TEL---     | Existing Telephone            |   |                           |
| —EW—          | Existing Edge of Wall         |   |                           |
| —EC—          | Existing Edge of Concrete     |   |                           |
| -----         | Construction and Paving Limit |   |                           |

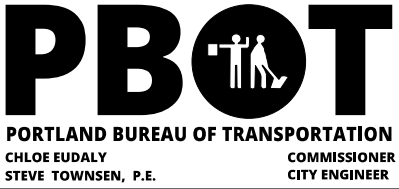
Plot Date: \$DATE\$ \$TIME\$ \$FILES\$

NO.	DATE	DESCRIPTION	APPD.
		REVISION	

DESIGNED BY <i>R. Bennett</i>	DATE APPROVED <i>June 2019</i>
CAD BY <i>P. Ferreira</i>	DIV. ENGINEER <i>S. Townsen</i>
CHECKED BY --	



APPROVALS:	
SUPERVISING ENGINEER	REG. PROF. ENGR. NO. 88470PE
CITY ENGINEER	REG. PROF. ENGR. NO. 51538PE



Public Street Improvements  
2025 S.E. 82nd Avenue  
S.E. 80th Avenue Frontage  
  
Cover Sheet

1/4 SECTION 3238
PROJECT NO. TB0093
SHEET NO. ST-01



1. ERRORS AND OMISSIONS ARE THE RESPONSIBILITY OF THE "ENGINEER OF RECORD". IF ERRORS OR OMISSIONS ARE FOUND AFTER THE PERMIT HAS BEEN ISSUED, THE PERMITTEE OR ITS CONTRACTOR SHALL CONTACT THE ENGINEER OF RECORD JIMI JOE OF CITY OF PORTLAND BUREAU OF TRANSPORTATION AT 503-823-7564 TO HAVE THE CORRECTIONS MADE. ALL CHANGES WILL REQUIRE THE APPROVAL OF THE CITY ENGINEER PRIOR TO THE WORK BEGINNING.
2. THE CONTRACTOR SHALL HAVE AT ALL TIMES ON-SITE, THE APPROVED CONSTRUCTION DRAWINGS & SPECIAL SPECIFICATIONS, CITY OF PORTLAND STANDARD SPECIFICATIONS & STANDARD DRAWINGS, AND ALL OTHER APPLICABLE SPECIFICATIONS BOOKS AND MANUALS. ELECTRONIC EQUIVALENT ARE ACCEPTABLE.
3. ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO THESE DRAWINGS AND THE APPLICABLE REQUIREMENTS OF THE 2010 EDITION OF THE CITY OF PORTLAND STANDARD CONSTRUCTION SPECIFICATIONS AND ALL REVISIONS AND SPECIAL SPECIFICATIONS.
4. A PRECONSTRUCTION CONFERENCE WITH CITY STAFF AND AN APPROVED TEMPORARY TRAFFIC CONTROL PLAN (ISSUED IN CONJUNCTION WITH A STREET /SIDEWALK CLOSURE PERMIT) ARE REQUIRED BEFORE COMMENCING WORK. SEE PERMIT FOR SCHEDULING A PRECONSTRUCTION CONFERENCE AND ACQUISITION OF THE TTP.
5. ELEVATIONS ARE BASED ON CITY OF PORTLAND DATUM FROM BENCH MARK NO. 1391, ELEVATION = 264.40, LOCATED AT THE SE CORNER OF 82ND AVE. AND YAMHILL STREET.
6. ATTENTION EXCAVATORS: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503.232.1987). IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CALL CENTER. YOU MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BUT NOT MORE THAN 10 BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION. CALL (811 OR 1-800-332-2344).

7. STREET FURNISHINGS ARE SHOWN FOR REFERENCE ONLY. THE INSTALLATION OF ALL STREET FURNITURE INCLUDING BUT NOT LIMITED TO BENCHES, NON CITY INSTALLED BIKE RACKS, GARBAGE CANS, ELECTRICAL SYSTEMS (CONDUIT, CONDUCTORS, OUTLETS), AND PUBLIC ART, ARE NOT AUTHORIZED UNDER THIS PERMIT. A SEPARATE REVOCABLE PERMIT IS REQUIRED.

8. REMOVE AND DISPOSE (AT A PROPER LOCATION OR LANDFILL) ALL MATERIALS EXCAVATED FROM WORK IN THE RIGHT-OF-WAY. FOR DISPOSAL ON PRIVATE PROPERTY, SECURE A FILL PERMIT, PRIOR TO BEGINNING WORK FROM THE BUREAU OF DEVELOPMENT SERVICES (BDS). PROVIDE A COPY OF THE APPROVED FILL PERMIT TO THE STREET CONSTRUCTION INSPECTOR.

FOR UNANTICIPATED CONTAMINATED MEDIA ENCOUNTERED, THE PERMITEE/APPLICANT OR ITS AGENT SHALL BE RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH THE MANAGEMENT, AND DISPOSAL OF CONTAMINATED MEDIA ENCOUNTERED. THE PERMITEE IS ALSO RESPONSIBLE FOR ALL RESULTANT DELAYS.

THE PERMITTEE OR S AGENT SHALL PROVIDE THE CITY (ENGINEERING AND INSPECTION) WITH COPIES OF ALL DISPOSAL PERMITS FROM THE PERMITTED DISPOSAL FACILITY, ANALYTICAL RESULTS USED TO GAIN ACCEPTANCE OF THE CONTAMINATED MEDIA, AND DISPOSAL RECEIPTS/DAILY WEIGH SLIPS. DAILY WEIGH SLIP AMOUNTS SHALL BE CHECKED AGAINST INSPECTOR'S DAILY REPORTS. THE PERMITTEE MUST USE AN OREGON FACILITY FOR DISPOSAL OF THE CONTAMINATED MEDIA.

9. UTILITIES SHOWN ON THESE PLANS ARE FOR INFORMATION AND COORDINATION PURPOSES ONLY AND ARE NOT AUTHORIZED FOR INSTALLATION UNDER THE PUBLIC STREET IMPROVEMENT PERMIT. PRIVATE AND PUBLIC UTILITY COMPANIES ARE REQUIRED TO SECURE SEPARATE UTILITY PERMITS FROM THE PBOT FOR ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY.

10. COORDINATION OF ALL UTILITY RELOCATES, REMOVALS, OR INSTALLATION WITHIN THE LIMITS OF WORK IS THE RESPONSIBILITY OF THE PERMITTEE/CONTRACTOR.

13. ALL MANHOLE LIDS AND VALVE BOXES SHALL BE ADJUSTED TO FINISHED STREET GRADE.

14. THE STREET INSPECTOR WILL MAKE THE FINAL DETERMINATION OF THE LIMITS OF PAVEMENT RESTORATION, INCLUDING SAWCUT LINES AND SKIN PATCHING. THE PERMITEE/CONTRACTOR SHALL CONSULT WITH THE STREET INSPECTOR PRIOR TO SAWCUTTING OR DEMOLISHING OF PAVEMENT.

15. SAWCUTS SHALL BE STRAIGHT MATCHLINES TO CREATE A BUTT JOINT BETWEEN THE EXISTING PAVEMENT AND NEW PAVEMENT AND ALL NEW PAVEMENT JOINTS SHALL BE SAND SEALED.

17. PAVEMENT SECTION SHALL BE AS SHOWN ON THE STREET TYPICAL SECTIONS.

18. UTILITY LIDS, MANHOLE COVERS, VALVE COVERS (THAT ARE NOT SHOWN ON THESE CONSTRUCTION DRAWINGS) ARE NOT ALLOWED IN THE THROUGH PEDESTRIAN ZONE. THEY MUST BE PLACED IN THE SIDEWALK FURNISHING OR BUILDING ZONE IF THE PROPER PBOT UTILITY PERMIT HAS BEEN SECURED.

19. FULL HEIGHT CURBS SHALL BE CONSTRUCTED AT ALL LOCATIONS UNLESS A NEW DRIVEWAY IS CONSTRUCTED AT THE SAME TIME AS THE CURB.

20. ALL SIDEWALK CONTRACTION JOINTS SHALL BE PER SECTION 00759.49 "CONTRACTION JOINTS" OF THE STANDARD CONSTRUCTION SPECIFICATION AND CITY STANDARD DWG P-551.

22. CONTRACTOR MAY USE CONCRETE OR ASPHALT CONCRETE FROM THE PRE-APPROVED MIX DESIGNS LIST IF AVAILABLE. IF NOT, THE CONTRACTOR WILL NEED TO SUBMIT A MIX DESIGN FOR APPROVAL.

23. USE ONLY APPROVED DETECTABLE WARNING DEVICES FROM THE CITY'S CONSTRUCTION PRODUCTS LIST (CPL).

24. ALL DRIVEWAYS ARE REQUIRED TO HAVE A MINIMUM OF 3 FEET OF HARD SURFACING BEHIND SIDEWALK (SEE STD DWG P-536).

28. THE CONTRACTOR MUST ACQUIRE AN APPROVED TEMPORARY STREET USE PERMIT (TSUP) PRIOR TO CLOSURE OF ANY STREET, SIDEWALK, TRAVEL LANE OR PARKING LANE. CONTACT CONTRACT MANAGER KEN FINCH OF CITY OF PORTLAND BUREAU OF TRANSPORTATION AT 503-823-2172 PRIOR TO ACQUIRING TSUP.
29. THE CONTRACTOR SHALL NOT REMOVE OR COVER ANY TRAFFIC CONTROL SIGNS, PAVEMENT MARKINGS, OR BARRICADES THAT ARE NOT IDENTIFIED ON THE APPROVED TEMPORARY TRAFFIC CONTROL PLAN.
30. THE CONTRACTOR SHALL MAINTAIN ALL NECESSARY TEMPORARY TRAFFIC CONTROL DEVICES (INCLUDING BUT NOT LIMITED TO THE FOLLOWING SIGNS, PAVEMENT MARKINGS, AND BARRICADES) UNTIL THE PERMANENT TRAFFIC CONTROL DEVICES ARE INSTALLED.
32. THE CONTRACTOR SHALL INSTALL OR REINSTALL ALL PERMANENT TRAFFIC CONTROL SIGNING, CURB AND PAVEMENT MARKINGS, AND BARRICADES.
33. THE CONTRACTOR SHALL SUBMIT MATERIALS LIST FOR APPROVAL 14 DAYS PRIOR TO INSTALLING PERMANENT TRAFFIC CONTROL SIGNING, CURB AND PAVEMENT MARKINGS, AND BARRICADES.
34. ALL NEW SIGN MATERIALS SHALL COMPLY WITH SECTION 2910 OF THE CITY OF PORTLAND STANDARD CONSTRUCTION SPECIFICATIONS. ALL SIGNS SHALL BE TYPE III OR IV BACKGROUND SHEETING ON ALUMINUM SIGN BLANKS. SIGN TYPES FOR EACH SIGN, AS SPECIFIED IN SECTION 2910.02, ARE NOTED IN THE PLANS.
35. SIGNS AND SIGN POSTS REMOVED BY THE PERMITEE OR ITS AGENT SHALL BE DELIVERED TO THE BUREAU OF MAINTENANCE, ALBINA YARD. CONTACT JIM BUHLER AT 503-823-4056 TO ARRANGE A DELIVERY TIME. REMOVE ALL SIGNS, CONCRETE AND DEBRIS FROM THE POST PRIOR TO DELIVERY.
39. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY CURB OR PAVEMENT MARKINGS DAMAGED, WORN OUT OR REMOVED DUE TO CONTRACTOR'S OPERATION.

42. ALL TREE REMOVAL SHALL COMPLY WITH THE FEDERAL MIGRATORY BIRD TREATY ACT. SEE THE SPECIAL PROVISIONS FOR REQUIREMENTS PRIOR TO CUTTING OF ANY TREE.

43. ALL GROUND DISTURBANCES NEAR TREES REQUIRES ROOT INSPECTION!!  
CONTACT URBAN FORESTRY (URBAN FORESTRY AT 503-823-8733; FOR ROOT  
INSPECTIONS PRESS 5) PRIOR TO ALL EXCAVATIONS ADJACENT TO TREES.  
CONSULTATION WITH THE URBAN FORESTER IS REQUIRED BEFORE CUTTING OF  
ROOTS.

44. SELECT TREE PLANTING SPECIES AS INSTRUCTED BY CITY OF PORTLAND URBAN FORESTRY AT 503-823-8733; TO LEAVE A MESSAGE FOR THE TREE INSPECTOR PRESS 5.

45. TREE PROTECTION FENCING MUST BE INSTALLED AND APPROVED PRIOR TO ANY GROUND DISTURBING ACTIVITY. CALL 823-TREE (8733) TO SCHEDULE THE TREE PROTECTION INSPECTION AND OBTAIN THE URBAN FORESTRY PERMIT.

46. IF ANY ROOTS OVER 2" IN DIAMETER ARE ENCOUNTERED A ROOT INSPECTION AND PERMIT IS REQUIRED PRIOR TO CUTTING. CALL CITY TREE INSPECTOR CASEY CLAPP AT 503-823-4467 FOR THE ROOT INSPECTION.

47. EROSION/SEDIMENTATION CONTROL (ESC) IS REQUIRED ON THIS PROJECT. IMPLEMENTATION OF THE ESC AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE PERMITEE OR ITS AGENT UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED. THE PERMITEE OR ITS AGENT SHALL PROVIDE INLET PROTECTION TO DOWNSTREAM INLETS FROM THE SITE PER THE EROSION CONTROL MANUAL [MARCH 2008]. DETAIL BASIN AND STORM DRAIN INLET PROTECTION SHALL BE INSTALLED PER DETAIL DRAWINGS 4.3-B AND 4.3-G.

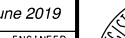
48. CONSTRUCTION OF ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH OAR 340-41-55 AND THE CITY OF PORTLAND EROSION CONTROL MANUAL [MARCH 2008].

49. APPROVAL OF THIS EROSION / SEDIMENTATION CONTROL (ESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF ANY OTHER PROPOSED PROJECT DESIGN ELEMENTS.

50. THE PERMITTEE OR ITS AGENT SHALL CLEARLY MARK WITH SURVEY FLAGGING THE CLEARING LIMITS OF THE WORK ZONE SHOWN ON THIS PLAN PRIOR TO BEGINNING EARTHWORK. DURING CONSTRUCTION, THE PERMITTEE OR ITS AGENT SHALL MAINTAIN THE MARKED CLEARING LIMITS AND SHALL NOT DISTURB THE AREAS OUTSIDE OF THE WORK ZONE.

51. THE ESC MEASURES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR THE ANTICIPATED SITE CONDITIONS. DURING CONSTRUCTION, THE PERMITTEE OR ITS AGENT SHALL INSPECT THESE MEASURES DAILY AND UPGRADE THEM TO ENSURE THAT SEDIMENT-LADEN WATER DOES NOT LEAVE THE WORK ZONE.

ALL OFF-SITE PUBLIC STORMWATER WILL BE MANAGED BY SEDIMENTATION AND SUMP MANHOLES IN S.E. HARRISON AND LINCOLN STREETS WEST OF S.E. 80TH AVENUE CONSTRUCTED BY PBOT LID PROJECT. ON-SITE PAVEMENT REPAIR IS INCIDENTAL AND WILL NOT IMPACT ON-SITE STORMWATER MANAGEMENT.

DESIGNED BY <b>R. Bennett</b>	DATE APPROVED <b>June 2019</b>	
CAD BY <b>P. Ferreira</b>	DIV. ENGINEER <b>S. Townsen</b>	
CHECKED BY <b>--</b>		


APPROVALS:

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SUPERVISING ENGINEER REG. PROF. ENGR. NO. 88470PE

---

CITY ENGINEER REG. PROF. ENGR. NO. 51538PE



**PORTLAND BUREAU OF TRANSPORTATION**

**CHLOE EUDALY**  
**STEVE TOWNSEN, P.E.**

**COMMISSIONER**  
**CITY ENGINEER**

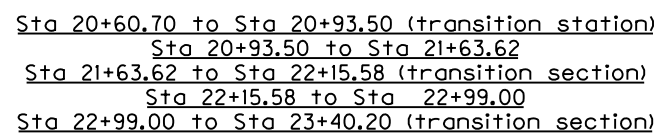


*Public Street Improvements  
2025 S.E. 82nd Avenue  
S.E. 80th Avenue Frontage*

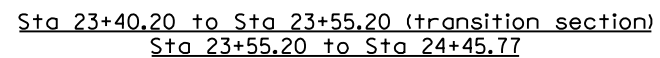
## General Construction Notes

1/4 SECTION
3238
PROJECT NO.
TB0093
SHEET NO.
ST-02








Typical Section 1  
Scale:  $\frac{3}{8}" = 1'-0"$  ST-04

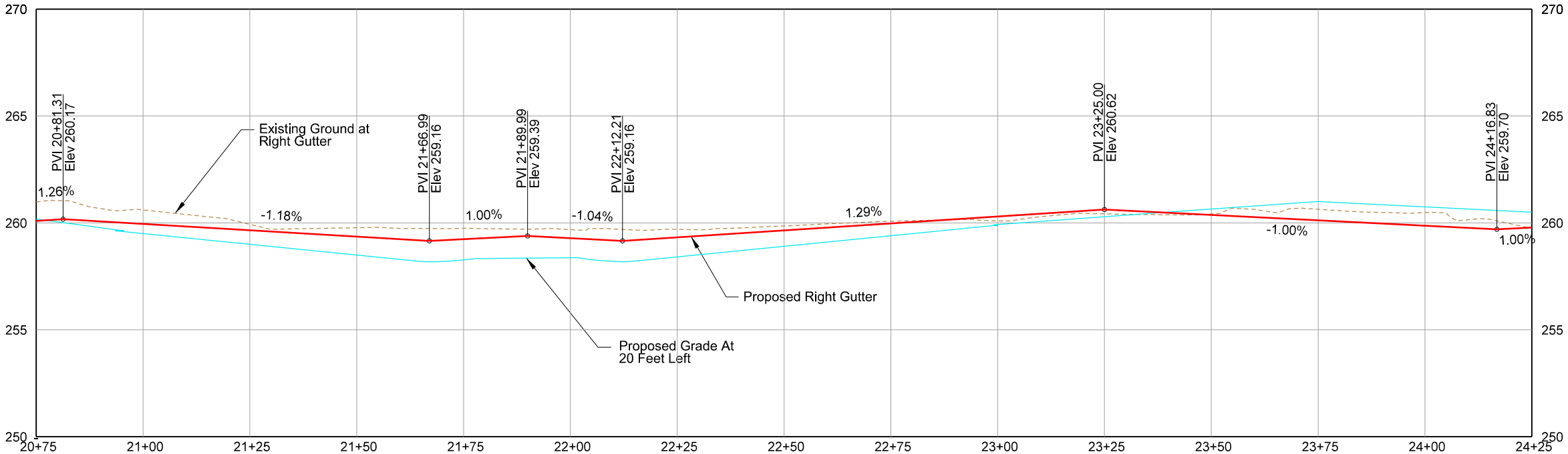


Typical Section 2  
Scale:  $\frac{3}{8}'' = 1'-0''$  ST-04

				DESIGNED BY <i>R. Bennett</i>	DATE APPROVED <i>June 2019</i>	 digital signature	APPROVALS:			 <b>PORTLAND BUREAU OF TRANSPORTATION</b> CHLOE EUDALY STEVE TOWNSEN, P.E.	 Public Street Improvements 2025 S.E. 82nd Avenue S.E. 80th Avenue Frontage	1/4" SECTION 3238
				CAD BY <i>P. Ferreira</i>	DIV. ENGINEER <i>S. Townsen</i>			SUPERVISING ENGINEER	REG. PROF. ENGR. NO. 88470PE			PROJECT NO. <i>TB0093</i>
				CHECKED BY --				CITY ENGINEER	REG. PROF. ENGR. NO. 51538PE			SHEET NO.
NO.	DATE	DESCRIPTION	APPD.									ST-03
REVISION						Expires 06/30/21						



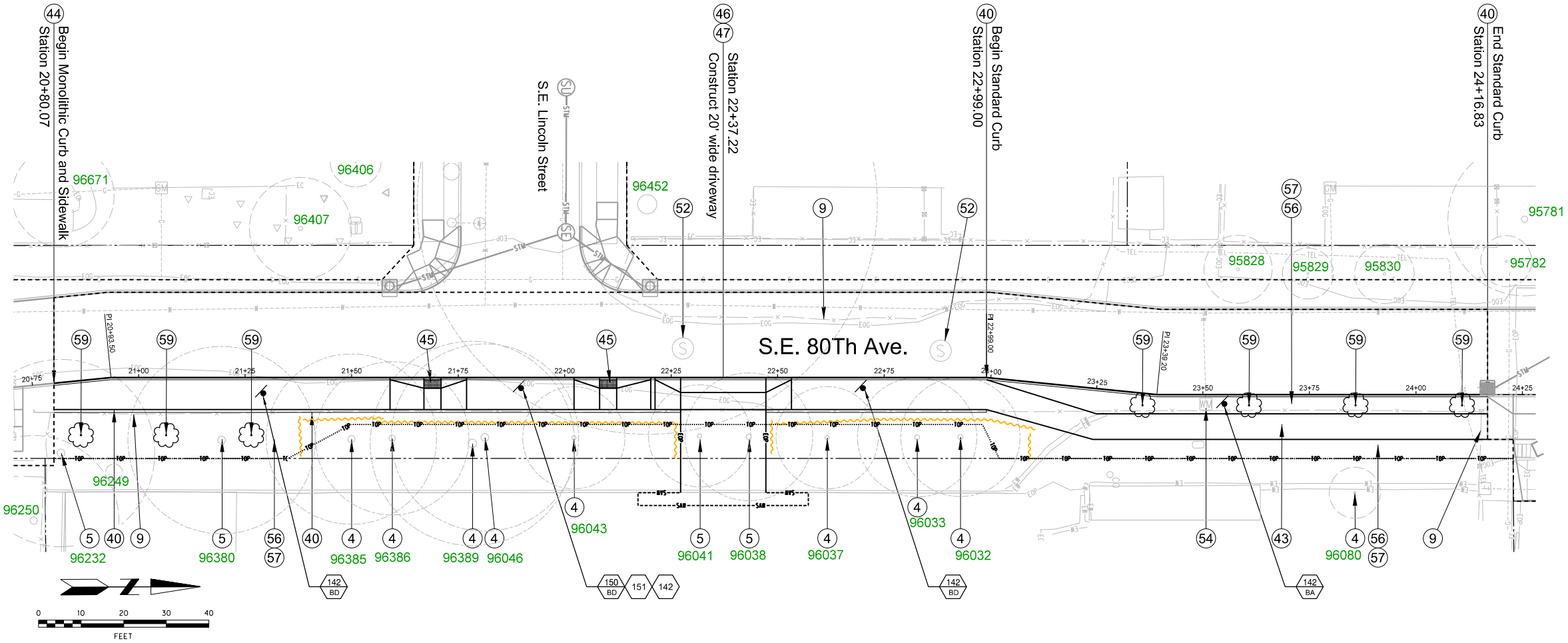
Profile Proposed Right Gutter S.E. 80th Ave.



- Signing Notes:**
- 1.) Refer to traffic sign bubble legend shown on sheet ST-06.
  - 2.) Refer to traffic sign & post data table shown on sheet ST-06
  - 3.) Refer to signing standard drawings P-400 thru P-420.

- Construction Notes**
- 4 Save tree - 9.  
Orange mesh fence 3½' tall is required.
  - 5 Remove tree - 4.
  - 9 Remove and salvage fence.
  - 40 Construct standard curb. See Std Drg No P-540.
  - 43 Construct concrete sidewalk - 4in thick.  
See Std Drg No P-551.
  - 44 Construct monolithic curb and sidewalk.  
See Sht ST-03 and Std Drg No P-551.
  - 45 Construct monolithic sidewalk ramp.  
See Std Drg Nos P-547, P-548 and RD759.
  - 46 Construct concrete driveway.  
See Sht ST-05 and Std Drg No P-528.
  - 47 Construct AC driveway connection.  
See Std Drg No P-536.
  - 52 Adjust manhole (minor) - 2. See Std. Drg. No. P-507
  - 54 Adjust box - 1.
  - 56 Install topsoil - 6in thick.
  - 57 Install lawn seeding.
  - 59 Plant tree -7.  
See Std. Drg. No. P-581

- Legend:**
- Construction by others PBOT LID Project
  - Tree protection is required around trees.  
Must be minimum of 3.5' tall orange  
construction fencing on t-stakes



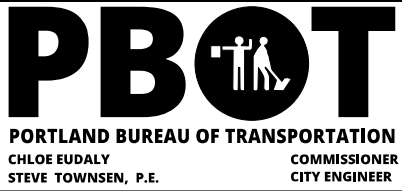
Plot Date: \$DATE\$ \$TIME\$  
Filename: \$FILE\$.  
\$TIME\$

NO.	DATE	DESCRIPTION	APPROD.
REVISION			

DESIGNED BY R. Bennett	DATE APPROVED June 2019
CAD BY P. Ferreira	DIV. ENGINEER S. Townsen
CHECKED BY --	



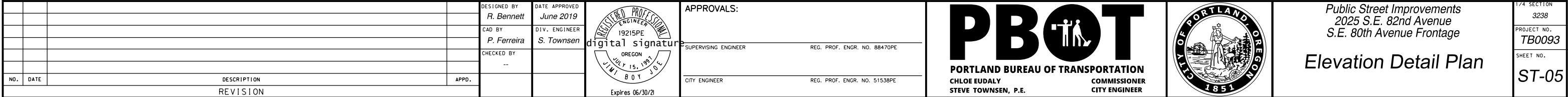
APPROVALS:	
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CITY ENGINEER	REG. PROF. ENGR. NO. 51538PE



Public Street Improvements  
2025 S.E. 82nd Avenue  
S.E. 80th Avenue Frontage  
  
Plan and Profile

1/4 SECTION 3238
PROJECT NO. TB0093
SHEET NO. ST-04







\_\_\_\_\_

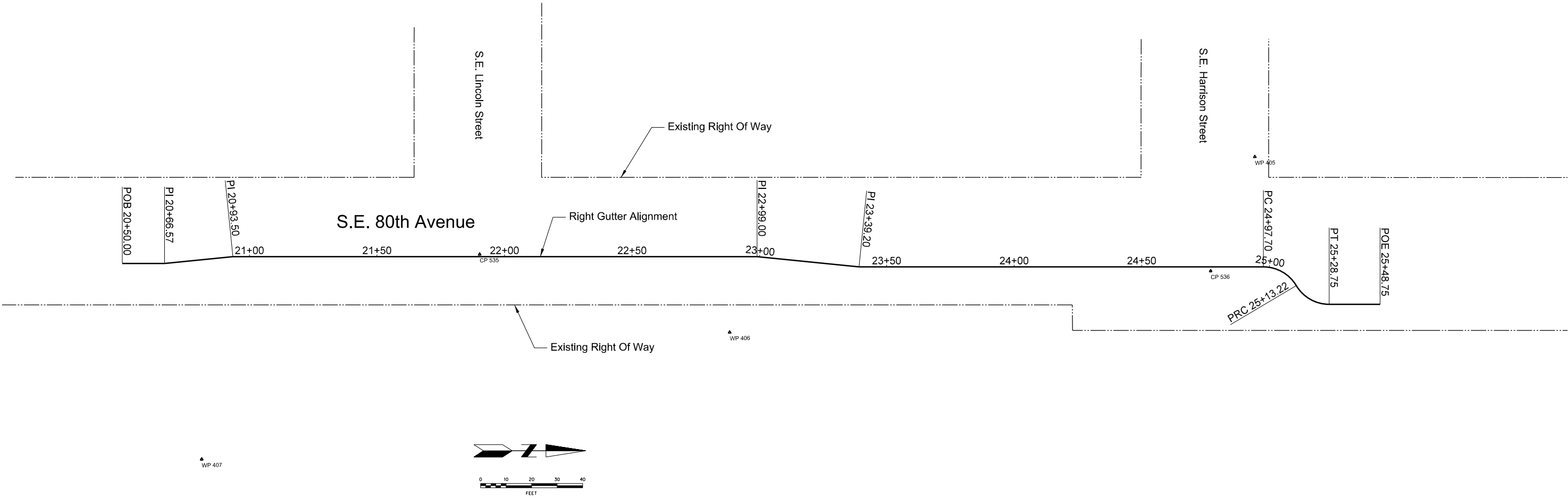


SURVEY CONTROL POINT DATA

405	7668591.02	678442.54	260.56
406	7668655.14	678235.16	262.65
407	7668700.24	678027.12	267.13
533	7668616.15	677885.45	264.74
535	7668622.39	678137.85	259.57
536	7668635.34	678424.22	260.29
534	7668268.29	677904.31	257.01

RIGHT GUTTER ALIGNMENT DATA

	NORTHING	EASTING	DIRECTION	DISTANCE
POB	677997.67	7668622.77	N 1°17'01.9999" E	16.57
PI	678014.24	7668623.14	N 4°25'36.1353" W	26.93
PI	678041.09	7668621.06	N 1°17'01.9999" E	205.50
PI	678246.54	7668625.67	N 6°59'40.1353" E	40.20
PI	678286.44	7668630.56	N 1°17'01.9999" E	158.50
PC	678444.90	7668634.12	S 88°42'58.0000" E	15.00
CC	678444.56	7668649.11	S 29°25'27.6866" E	15.00
PRC	678457.63	7668641.74	S 29°25'27.6866" E	15.00
CC	678470.69	7668634.37	S 88°42'58.0000" E	15.00
PT	678470.35	7668649.37	N 1°17'01.9999" E	20.00
POE	678490.35	7668649.82		



Plot Date: \$DATE\$      \$TIME\$      Filename: \$FILE\$.

NO.	DATE	DESCRIPTION	APPD.	
REVISION				

DESIGNED BY <i>R. Bennett</i>	DATE APPROVED <i>June 2019</i>
CAD BY <i>P. Ferreira</i>	DIV. ENGINEER <i>S. Townsen</i>
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digital signature

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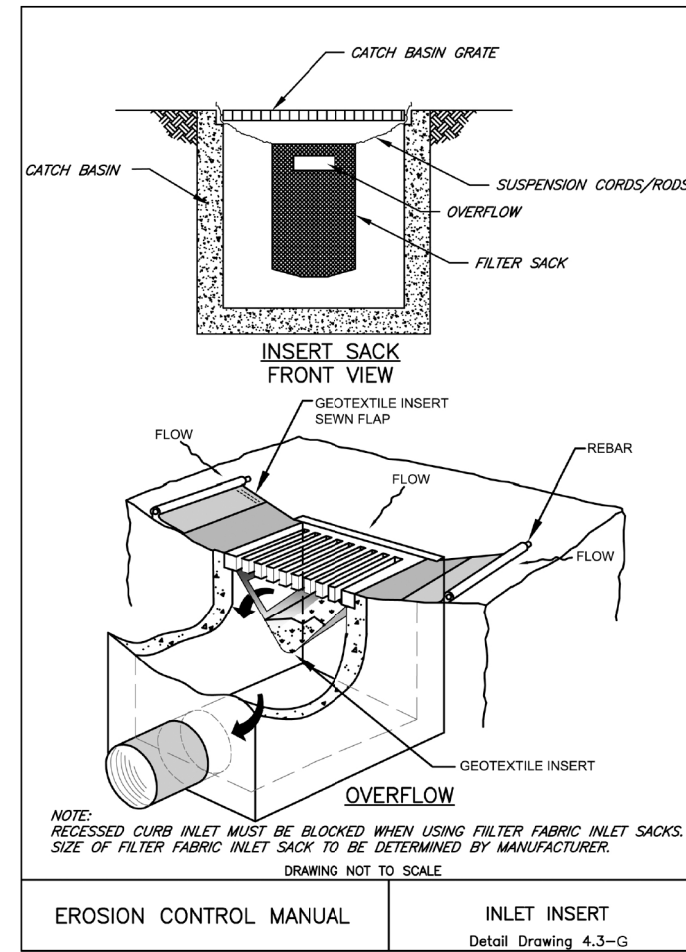


*Public Street Improvements  
2025 S.E. 82nd Avenue  
S.E. 80th Avenue Frontage*

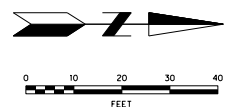
**Horizontal Alignment Plan**

1/4 SECTION 3238
PROJECT NO. <b>TB0093</b>
SHEET NO. <b>ST-07</b>





- Install Inlet Protection per  
Detail Drawing 4.3-B and 4.3-G this sheet



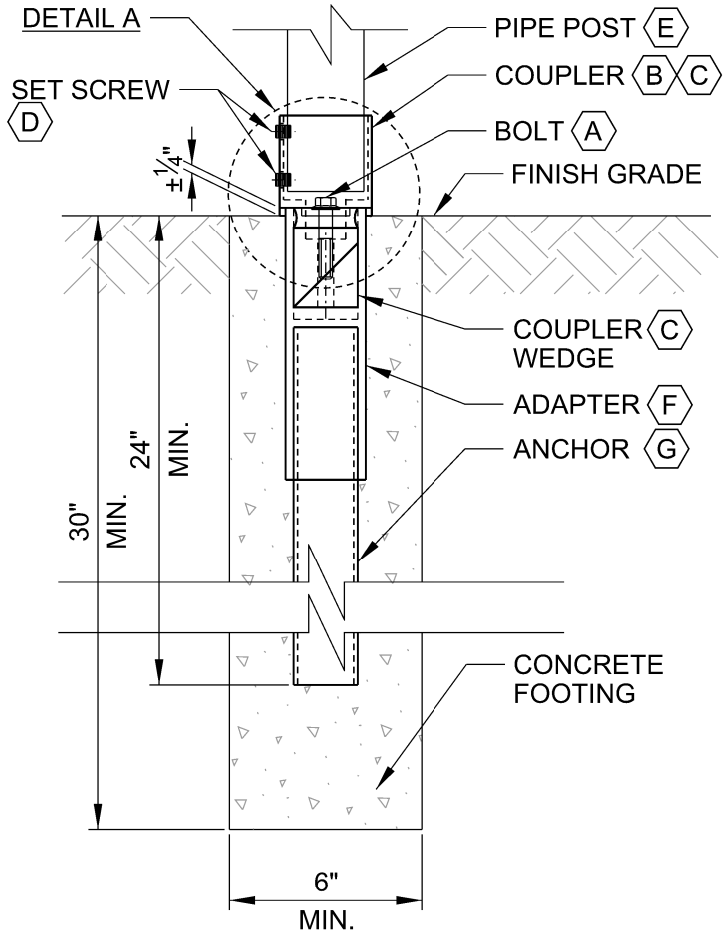
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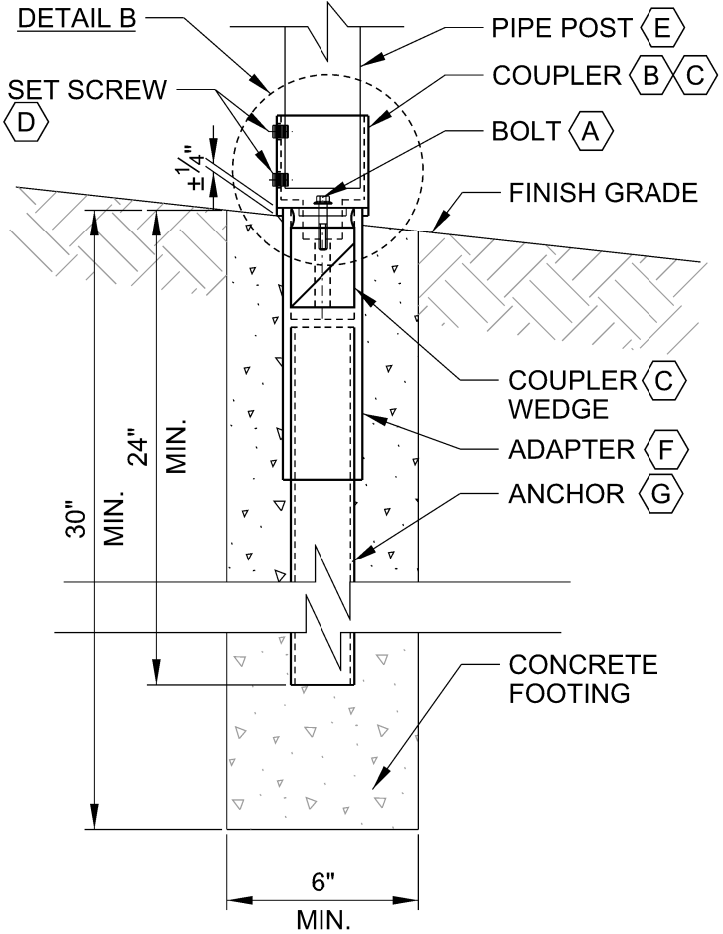
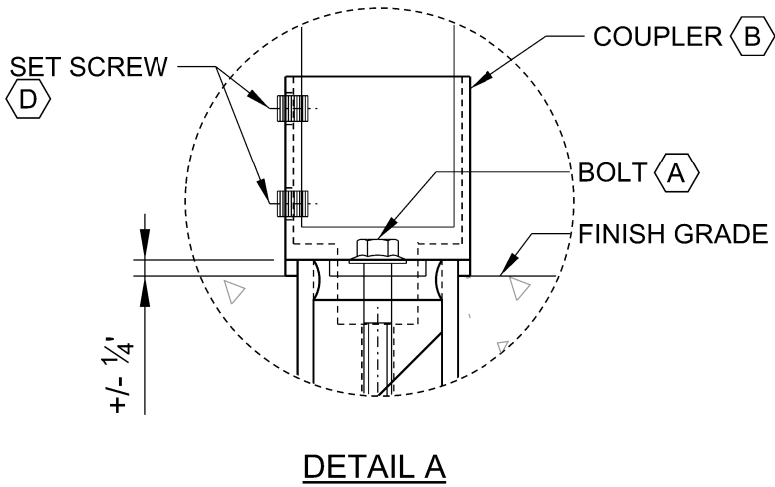
P-400



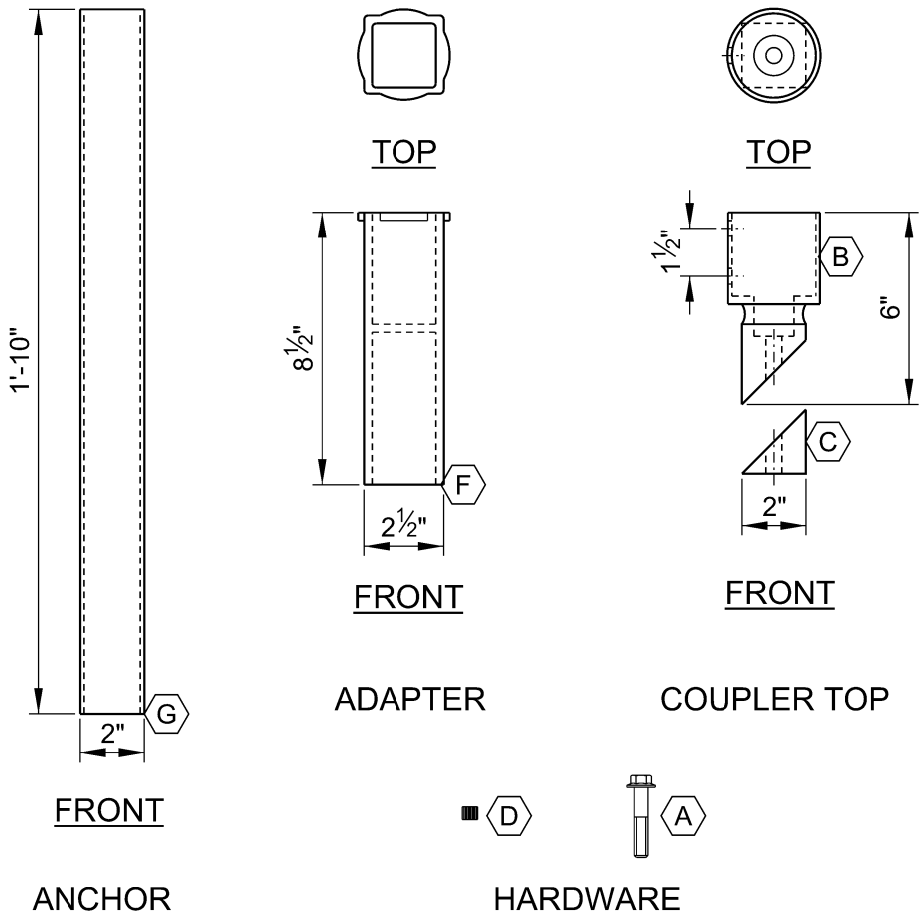
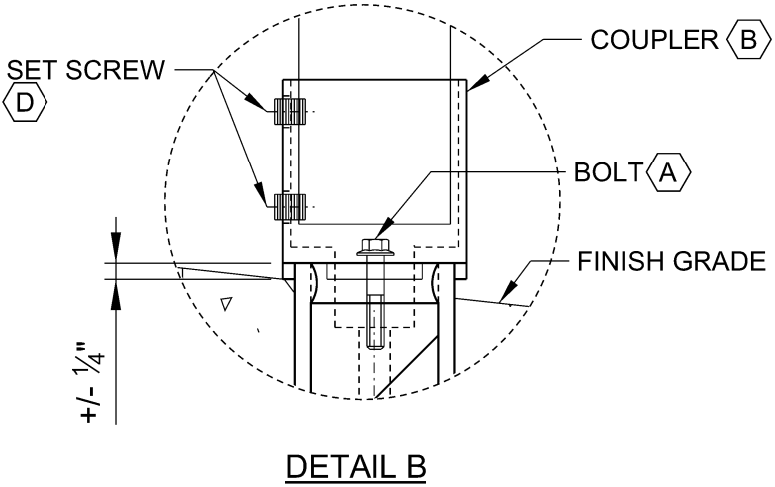
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**BREAKAWAY ANCHOR ASSEMBLY  
LEVEL GRADE INSTALLATION**



**BREAKAWAY ANCHOR ASSEMBLY  
SLOPED GRADE INSTALLATION**




ITEM NO.	DESCRIPTION	MATERIAL	QTY
(A)	1/2" X 2 1/4" USS HEX HEAD BOLT	GRADE 8 GALV. STEEL	1
(B)	2 3/8" COUPLER TOP (ROUND)	CAST IRON	1
(C)	COUPLER WEDGE (2")	CAST IRON	1
(D)	1/2" USS SET SCREW	STAINLESS STEEL	2
(E)	ROUND POST 2.375" O.D.	SCHEDULE 40 STEEL	1
(F)	ADAPTER 2.5" O.D. X 8.5"	CAST IRON	1
(G)	ANCHOR 2.0" O.D. X 22"	STEEL	1

**BREAKAWAY ANCHOR PARTS LIST**

- NOTES:**
1. FOR PIPE SIGN POST INSTALLATION IN EARTH.
  2. CENTER BOLT 'A' AND SET SCREWS 'D' SHALL BE TIGHTENED SECURELY SUCH THAT THE ENTIRE ASSEMBLY IS TIGHT.
  3. FOR LEVEL INSTALLATIONS:
    - THE ANCHOR 'G' AND ADAPTER 'F' SHALL BE PLACED SUCH THAT THE BOTTOM OF THE LIP SURROUNDING THE TOP OF ADAPTER IS AT FINISH GRADE.
  4. FOR SLOPED INSTALLATIONS:
    - THE ANCHOR 'G' SHALL BE PLACED AT TOP OF FINISH GRADE RELATIVE TO THE UPPER SIDE OF THE SLOPE SUCH THAT NO PART OF THE ANCHOR IS BELOW FINISH GRADE AT ANY POINT.
  5. EXTREME CARE SHALL BE TAKEN TO ENSURE THE ANCHOR ASSEMBLY IS PLACED VERTICALLY IN THE GROUND. THE ENTIRE SIGN INSTALLATION SHALL BE PLUMB AND TIGHT WHEN INSTALLATION IS COMPLETE.
  6. FOR OTHER INSTALLATION DETAILS FOLLOW MANUFACTURER'S INSTRUCTIONS.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.



BUREAU OF TRANSPORTATION  
CITY OF PORTLAND, OREGON  
*Steve Tournier*  
Chief Engineer

Standard Drawing Title

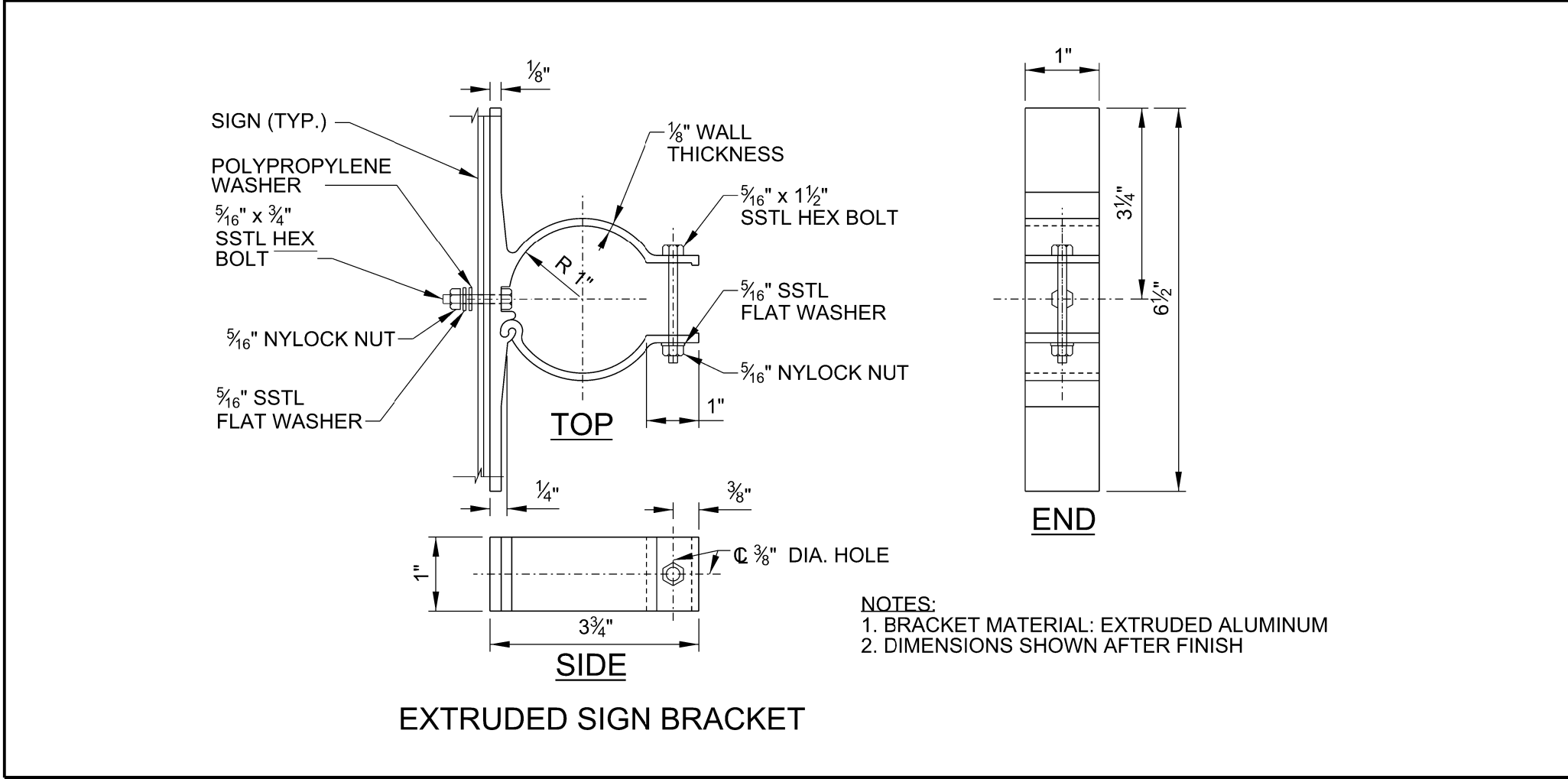
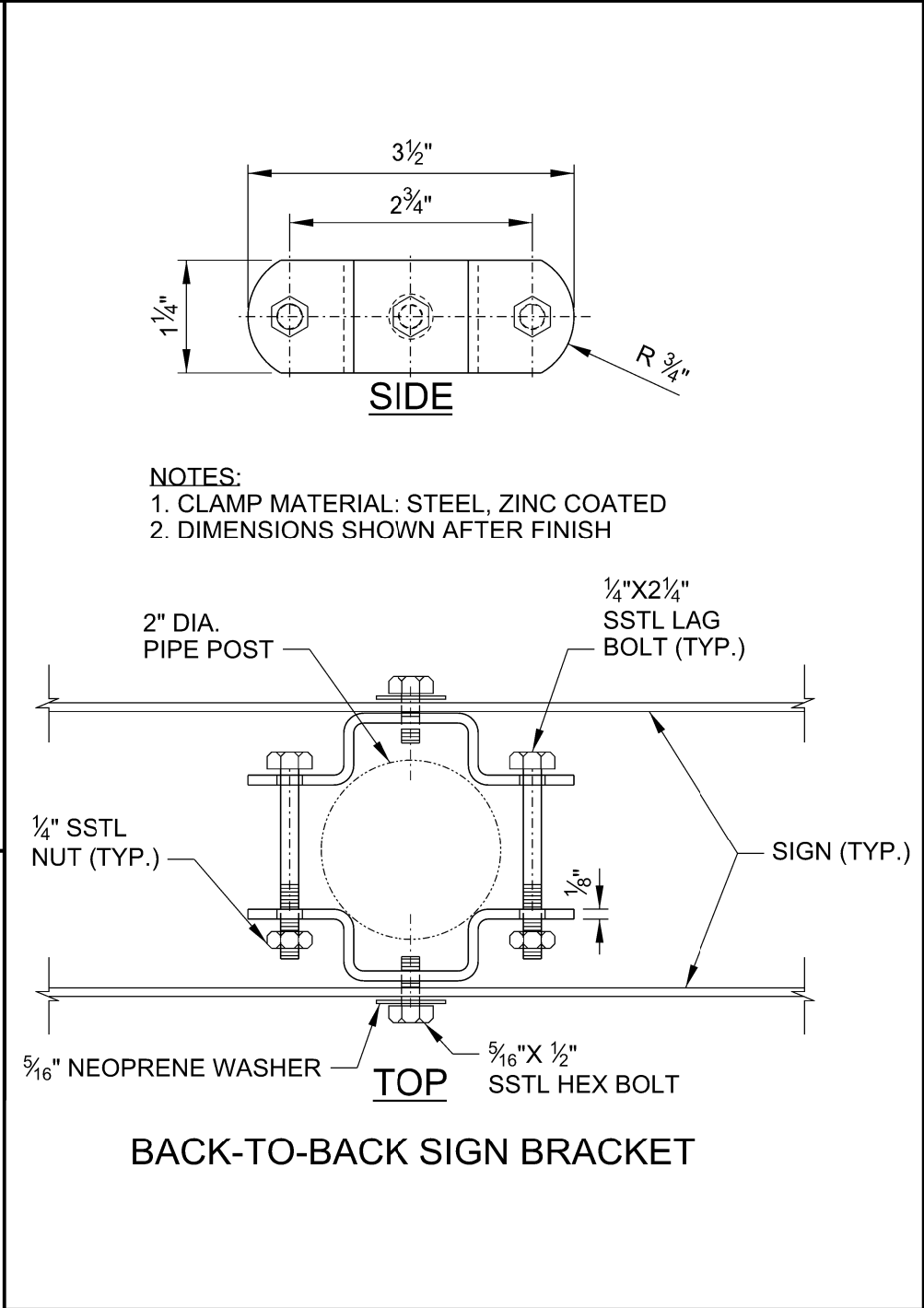
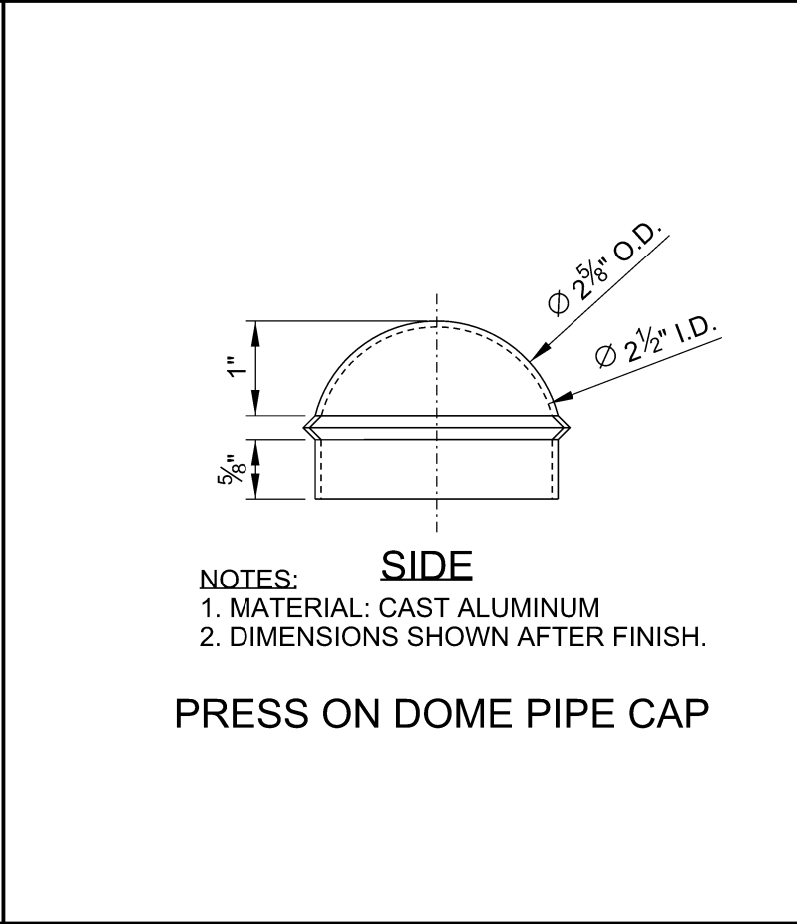
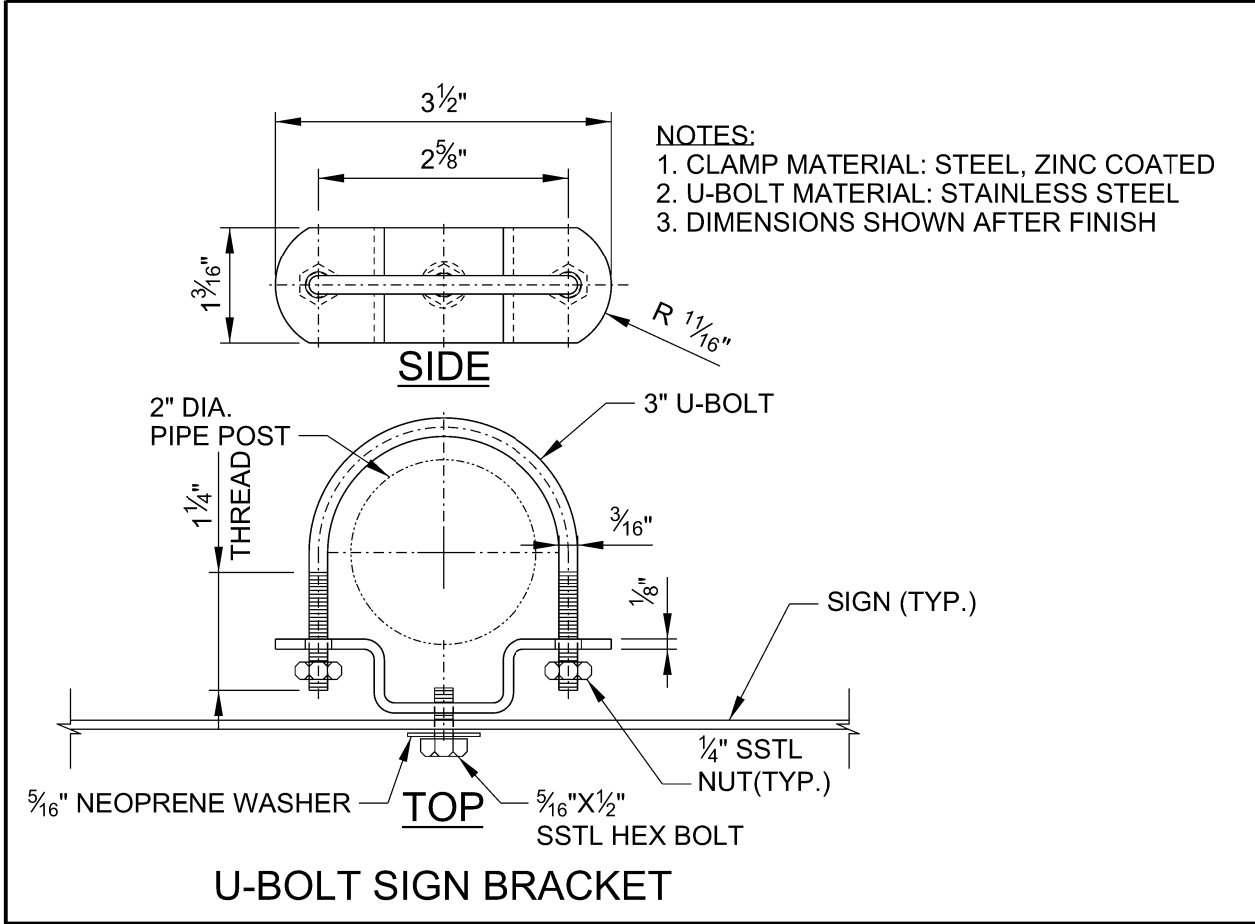
**BREAKAWAY ANCHOR  
TRAFFIC SIGN SUPPORTS**


Note:  
All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.

Effective Date: 04-18-11	Standard Drawing No. <b>P-405</b>
Calc. Book No.:	
Baseline Report Date:	



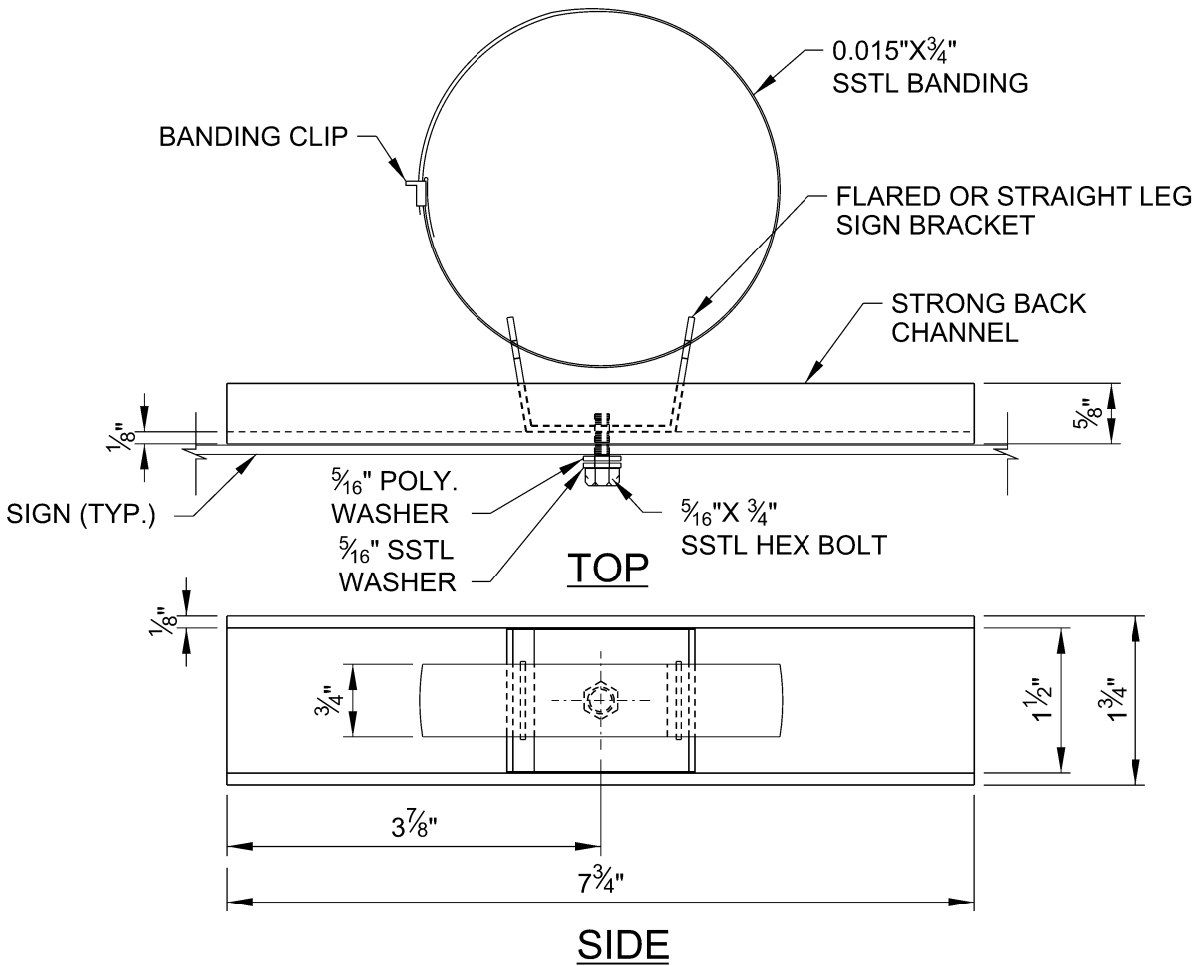
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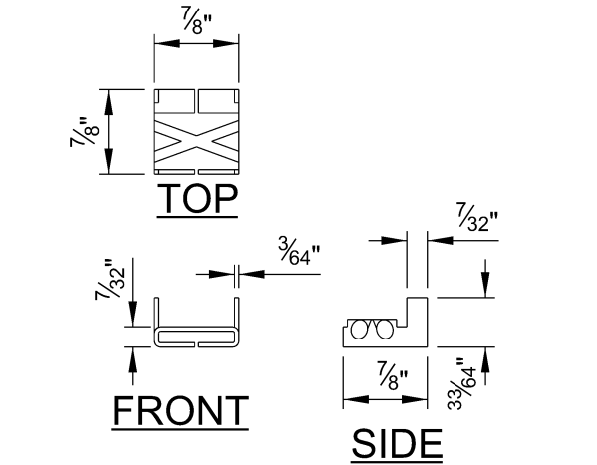
The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.	<div><div>BUREAU OF TRANSPORTATION CITY OF PORTLAND, OREGON</div><div><i>Steve Touman</i> Chief Engineer</div></div>		
	Standard Drawing Title  SIGN BRACKET, CAP DETAILS		
Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.	Effective Date: 03-17-11		Standard Drawing No.  P-410
	Calc. Book No.:		
	Baseline Report Date:		



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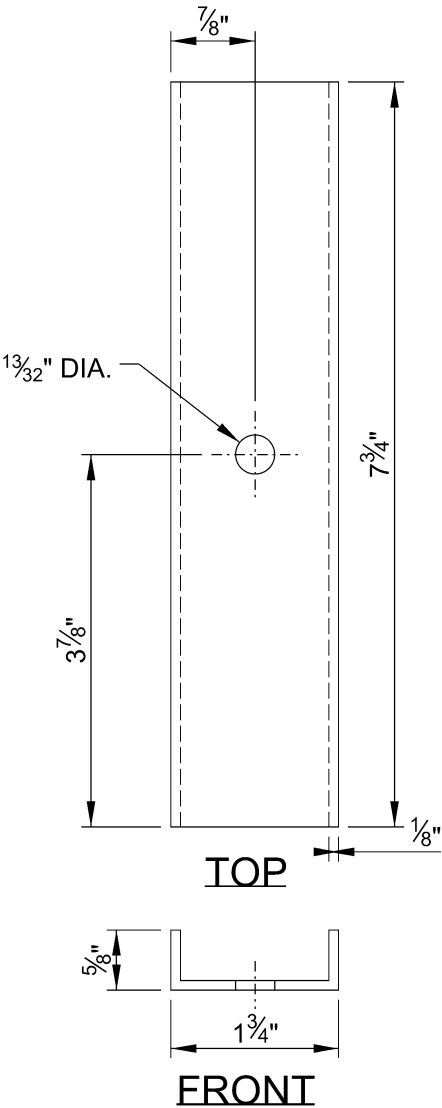


SIGN MOUNTING ASSEMBLY  
(FOR WOOD OR METAL POLES)



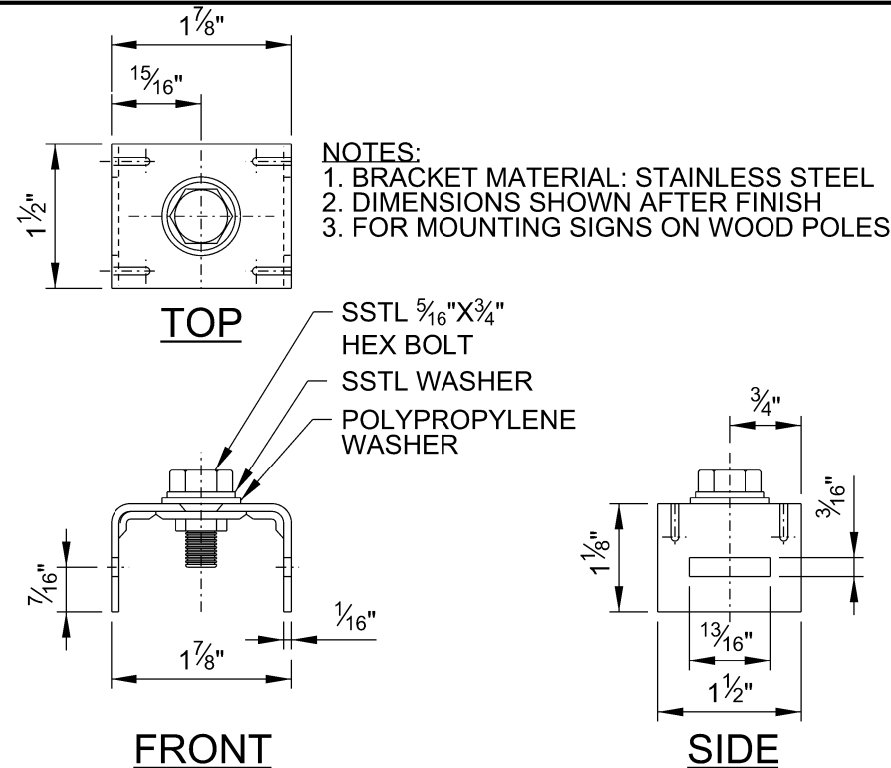
SIGN BRACKET BANDING CLIP

NOTES:  
1. CLIP MATERIAL: STAINLESS STEEL  
2. DIMENSIONS SHOWN AFTER FINISH

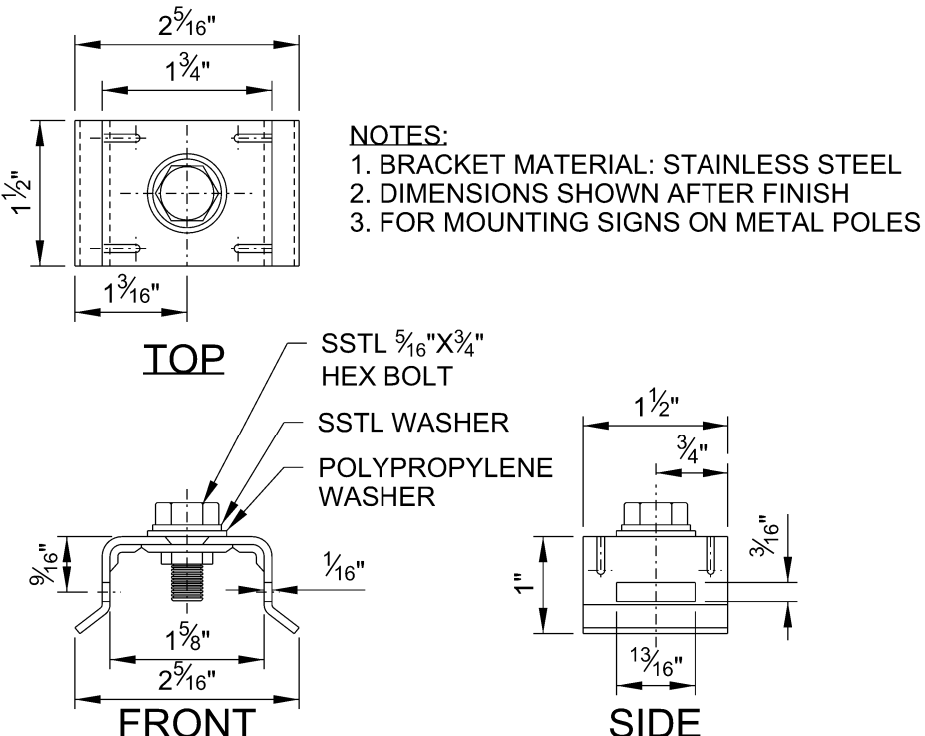


STRONG-BACK CHANNEL

NOTES:  
1. BRACKET MATERIAL: EXTRUDED ALUMINUM.  
2. DIMENSIONS SHOWN AFTER FINISH



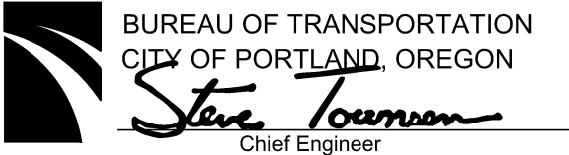
STRAIGHT LEG SIGN BRACKET



FLARED LEG SIGN BRACKET

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.

Note:  
All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.



Standard Drawing Title

SIGN BRACKET,  
STRONG-BACK DETAILS

Effective Date: 03-17-11

Calc. Book No.:

Baseline Report Date:

Standard Drawing No.

P-411



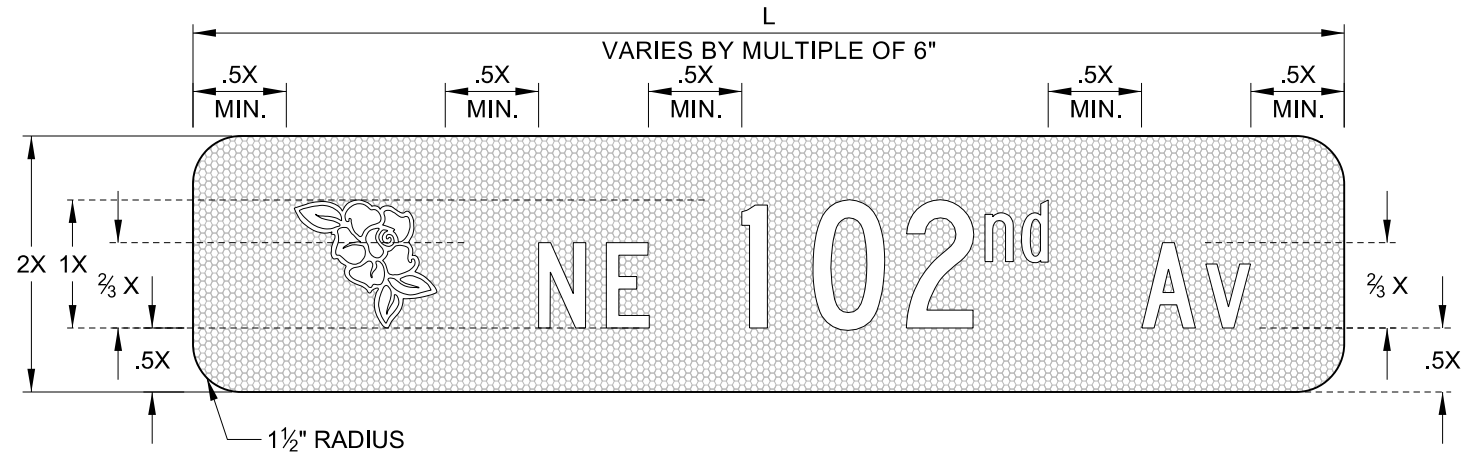
NOTES:

1. STREET NAME SIGNS SHALL BE MANUFACTURED WITH 0.125"THICK RECTANGULAR FLAT ALUMINUM BLADES.
2. STREET NAME SIGNS SHALL BE DOUBLE SIDED WHEN CANTILEVER MOUNTED OR CENTER MOUNTED ON TOP OF A PIPE SIGN POST, UNLESS APPROVED BY THE ENGINEER. OVERHEAD STREET NAME SIGNS SHALL BE SINGLE SIDED.
3. LEGENDS SHALL BE HIGHWAY GOTHIC SERIES C FONT. HIGHWAY GOTHIC SERIES B FONT MAY BE USED WITH ENGINEER'S APPROVAL WHEN THE MAXIMUM SIGN LENGTH WOULD OTHERWISE BE VIOLATED.
4. THE CITY'S "ROSE" LOGO SHALL BE USED ON OVERHEAD STREET NAME SIGNS ONLY. THE "ROSE" LOGO DESIGN SHALL BE OBTAINED FROM THE CITY OF PORTLAND OR AN APPROVED OUTSIDE SOURCE. THE LOGO MAY BE OMITTED ON OVERHEAD STREET NAME SIGNS WITH ENGINEER'S APPROVAL WHEN THE MAXIMUM SIGN LENGTH WOULD OTHERWISE BE VIOLATED. FOR GROUND MOUNTED STREET NAME SIGNS, DELETE THE "ROSE" LOGO AND THE SPACE BETWEEN THE LOGO AND LETTERING.
5. ACCEPTABLE ABBREVIATIONS PER MUTCD MAY BE USED EXCEPT FOR THE STREET NAME ITSELF.
6. FOR G5521V SIGNS WHERE TWO STREET NAMES ARE REQUIRED, THE SIGN HEIGHT SHALL BE 32 INCHES WITH A 1 INCH DIVIDING THE LINE BETWEEN STREET NAMES.
7. SHOP DRAWINGS FOR STREET NAME SIGN GRAPHICS MUST BE SUBMITTED TO THE PBOT TRAFFIC ENGINEER PRIOR TO SIGN FABRICATION.
8. STREET NAME SIGN MOUNTING:  
8" AND 12" HEIGHT STREET NAME SIGNS CAN BE MOUNTED ON A PIPE POST WITH CROSS PIECE HARDWARE (PBOT STD. DRG. P412), OR CANTILEVER MOUNTED ON A SIGNAL OR STREET LIGHT POLE (PBOT STD. DRG. P-413). FOR GROUND MOUNT STREET NAME SIGN PLACEMENT DETAILS SEE PBOT P-400.
- FOR 16" HEIGHT OVERHEAD STREET NAME SIGN MOUNTING ON MAST ARMS OR SIGNAL POLE RISERS, SEE PBOT STD. DRG. P-629.

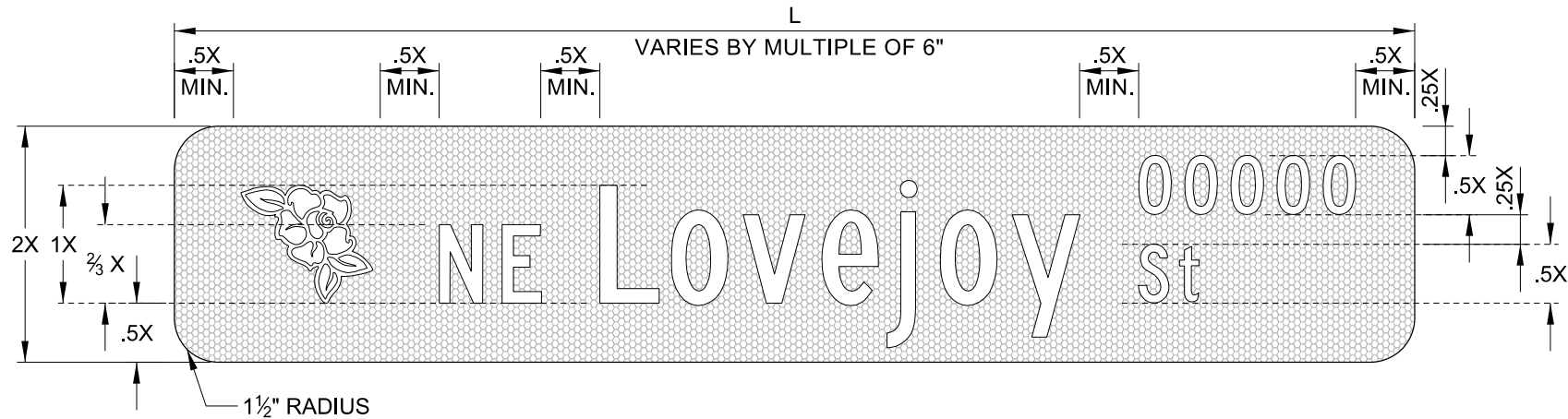
DIMENSIONING TABLE

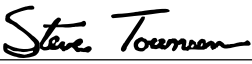
SIGN LOCATION	POSTED SPEED (MPH)	NUMBER OF LANES	PRIMARY LETTER HEIGHT (IN.) X	MAXIMUM SIGN LENGTH (IN.) L	SIGN CODE		SIGN TYPE
					DOUBLE SIDED	SINGLE SIDED	
OVERHEAD SIGNALIZED INTERSECTION	ALL	ALL	8	84	N/A	G5521V	"G5"
GROUND MOUNT	>40MPH	4+	6	54	G5506	G5506A	"G"
		2-3	4	48	G5501	G5501A	"G"
	≤40MPH	ALL	4	48	G5501	G5501A	"G"

NUMBERED STREET

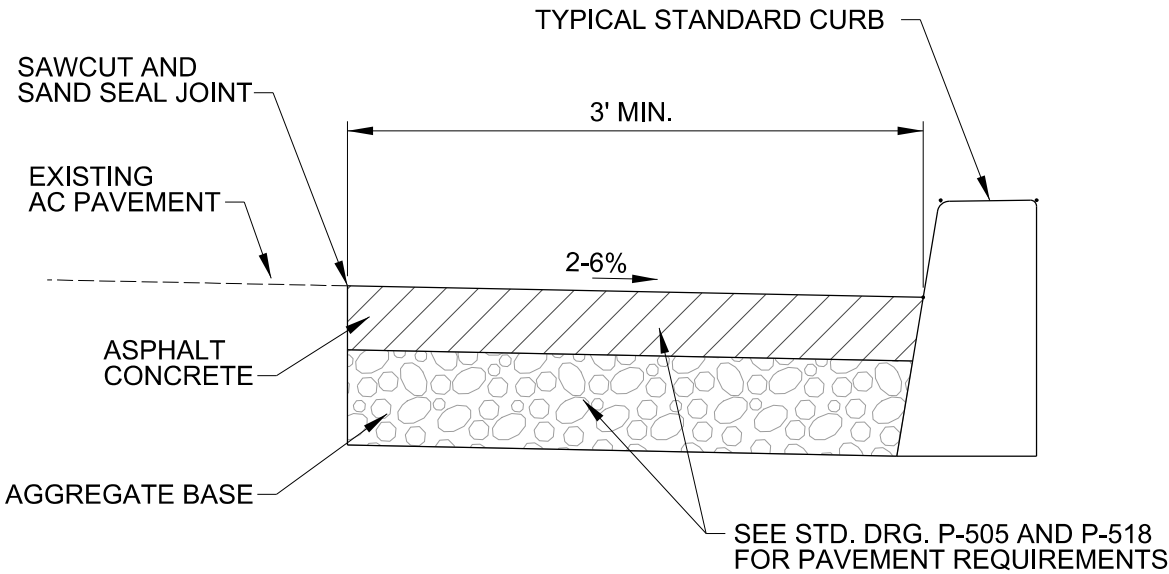


NON-NUMBERED STREET

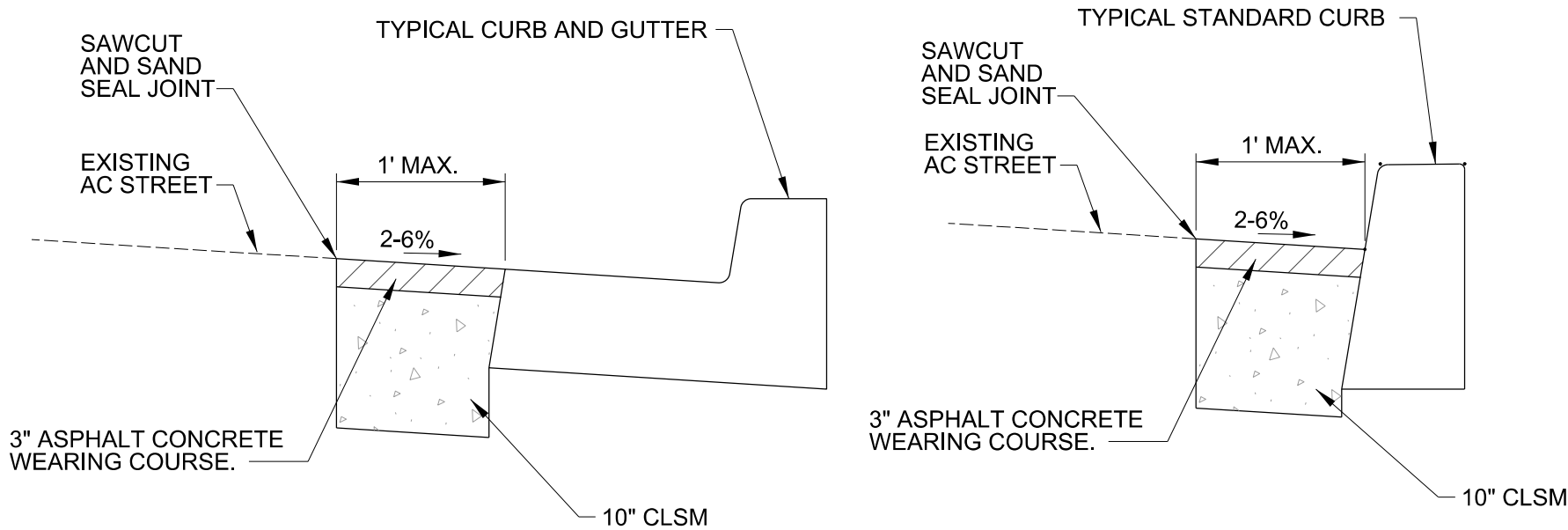


The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.	<b>PBOT</b> PORTLAND BUREAU OF TRANSPORTATION	
	 City Engineer	
	Standard Drawing Title	
	STREET NAME SIGN DETAIL	
Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.	Effective Date:	01-02-2019
	Calc. Book No.:	n/a
	Std. Drg. Report Date:	01-02-2019
Standard Drawing No.		P-420

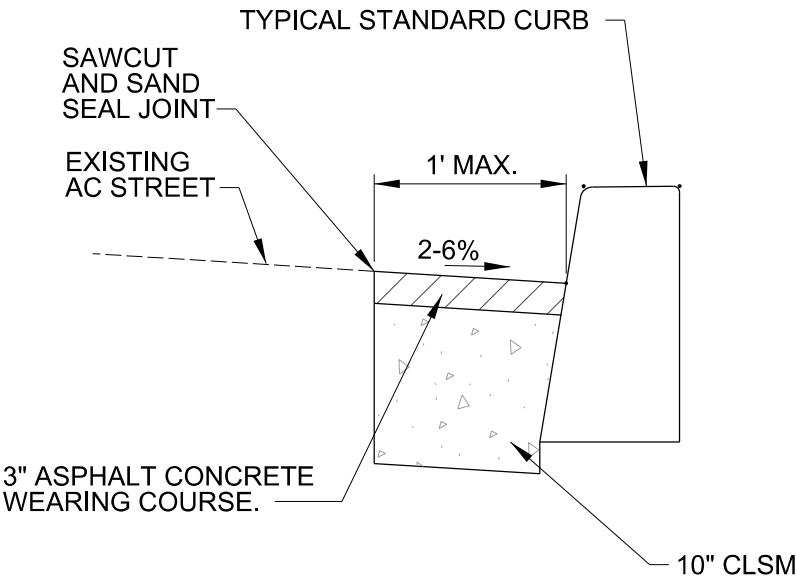




**TYPICAL PAVEMENT REPAIR**  
(SEE NOTES 8,9,12)



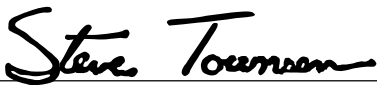
**TYPICAL CURB AND GUTTER**



**TYPICAL STANDARD CURB**

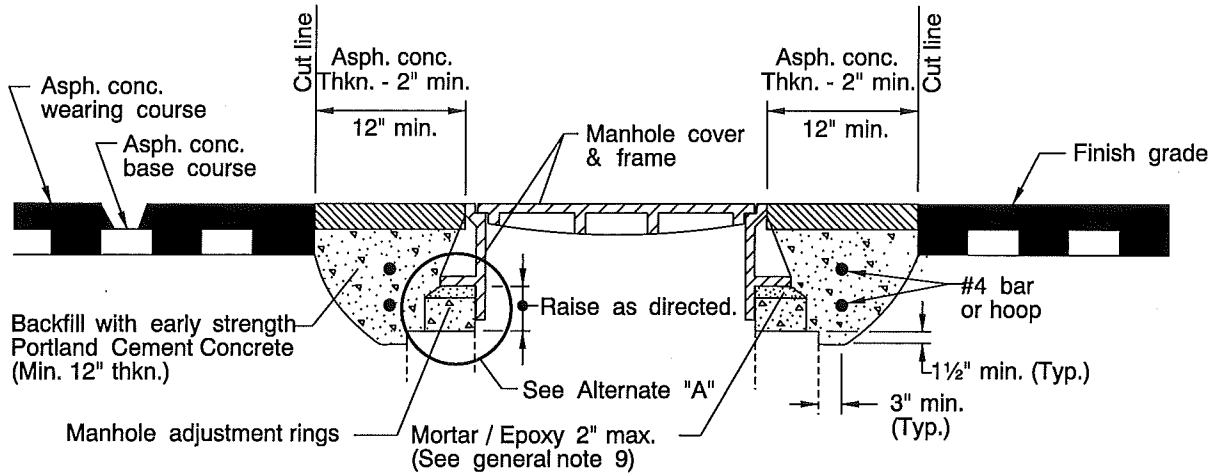
**ALTERNATE PAVEMENT REPAIR**  
(LOCAL STREETS ONLY)

- NOTES:**
1. LOCAL STREETS ARE DEFINED AS RESIDENTIAL STREETS WITH NO LANE MARKINGS AND NOT A TRANSIT BUS ROUTE. BUSY STREETS ARE DEFINED AS ALL OTHER.
  2. APPLIES TO PARTIAL STREET IMPROVEMENTS WHERE NEW CURB IS BEING CONSTRUCTED OR WHERE EXISTING CURBS ARE BEING RECONSTRUCTED.
  3. SAWCUTS OVER 1 FOOT SHALL BE WIDENED TO 3 FEET MINIMUM.
  4. SAWCUT 1 FOOT MINIMUM FROM EDGE OF EXISTING PAVEMENT WHEN NEW CURB IS BEING CONSTRUCTED.
  5. SAWCUT DIMENSION MEASURED FROM FACE OF STANDARD CURB OR FACE OF GUTTER PAN FOR COMBINATION CURB AND GUTTER.
  6. SEE STD. DRG. P-540 AND P-544 FOR CURB AND CURB DETAILS.
  7. MATCH EXISTING STREET CROSS SLOPE UP TO 6% MAX.
  8. PAVEMENT REPAIR ADJACENT TO COMMERCIAL DRIVEWAYS SHALL BE 3 FEET MIN.
  9. PAVEMENT REPAIR ADJACENT TO BIKE LANES SHALL EXTEND TO THE FULL WIDTH OF THE BIKE LANE.
  10. REPLACE PAVEMENT MARKINGS EITHER REMOVED, DAMAGED, OR COVERED BY SAND SEAL TACK.
  11. USE LEVEL 2 PG 64-22 ON ALL LOCAL AC STREETS. USE LEVEL 3 PG 70-22 ON ALL OTHER AC STREETS.
  12. PAVEMENT REPAIR ON BUSY STREETS SHALL EXTEND TO THE NEAREST LANE LINE OR CENTER OF TRAVEL LANE.

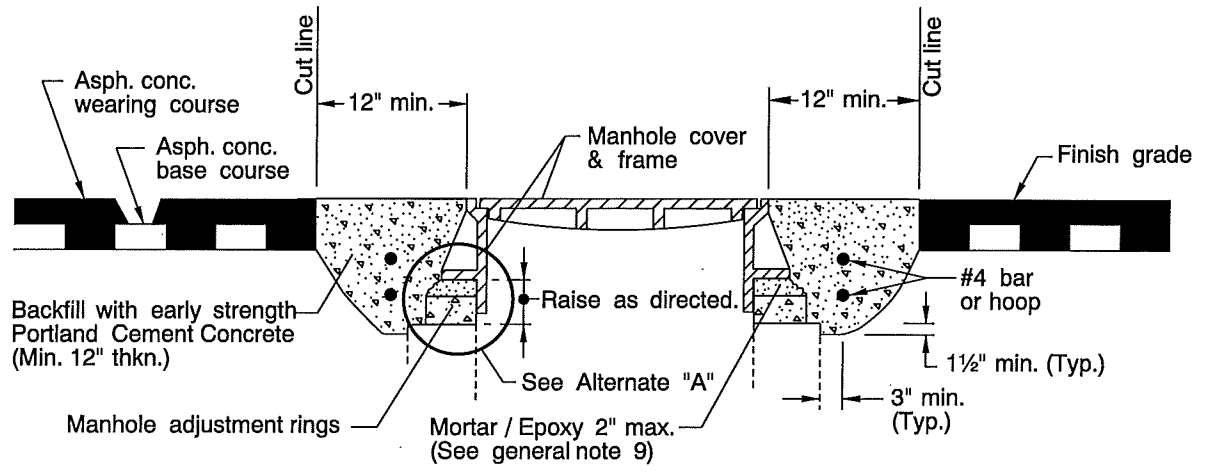
The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.	<b>PBOT</b> PORTLAND BUREAU OF TRANSPORTATION	
	 City Engineer	
	Standard Drawing Title	
	ASPHALT CONCRETE PAVEMENT REPAIR ADJACENT TO CURB	
Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.	Effective Date:	02-26-2018
	Calc. Book No.:	506
	Std. Drg. Report Date:	-----
Standard Drawing No.		P-506



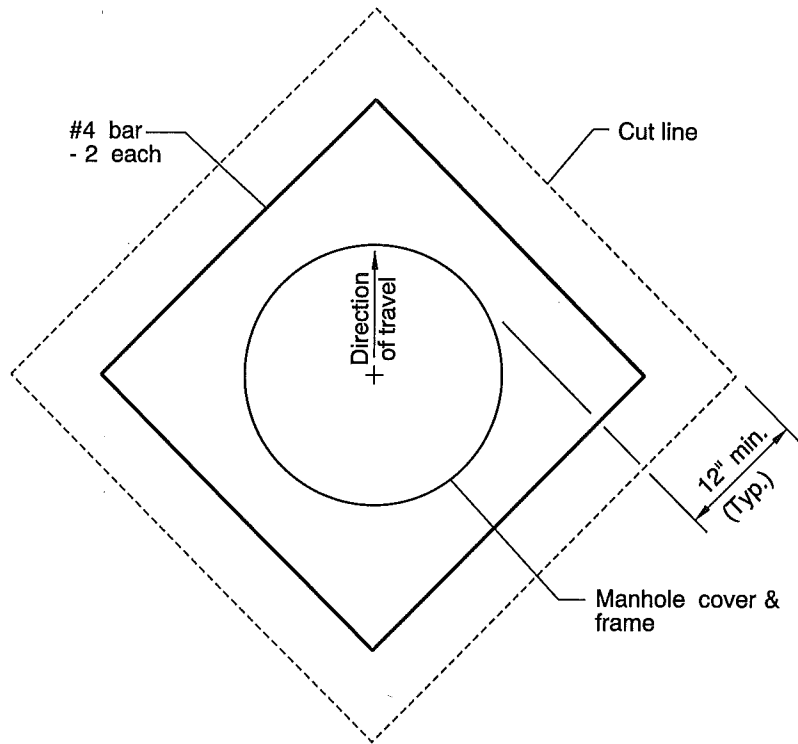
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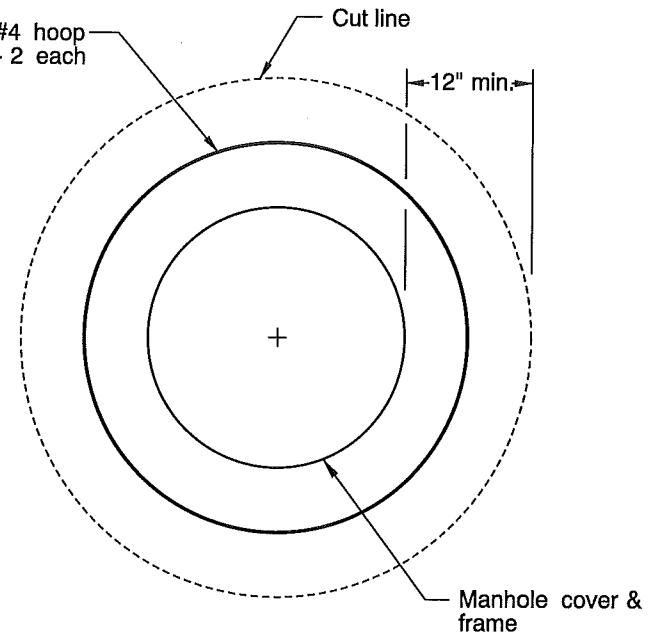
METHOD "A"



METHOD "B"



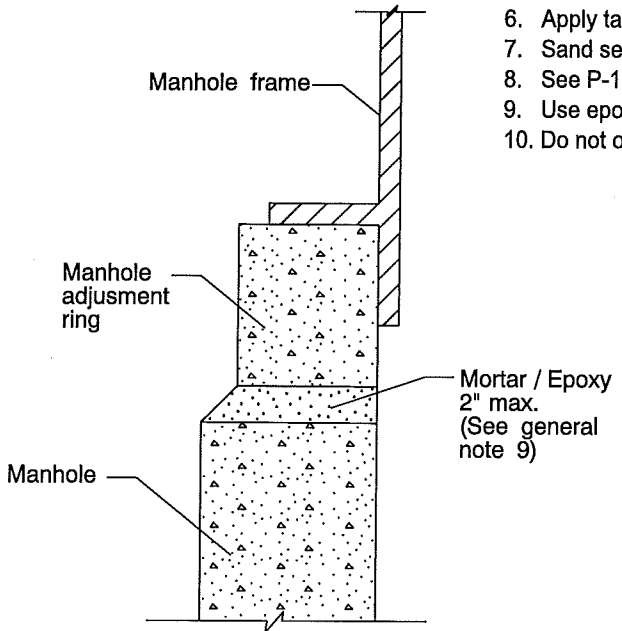
PLAN  
SQUARE CUT




PLAN  
CIRCULAR CUT

GENERAL NOTES FOR ALL DETAILS:

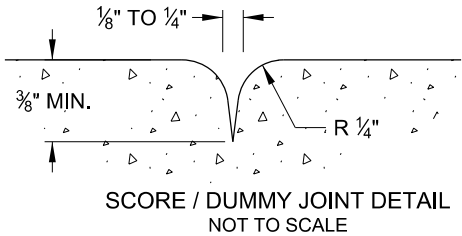
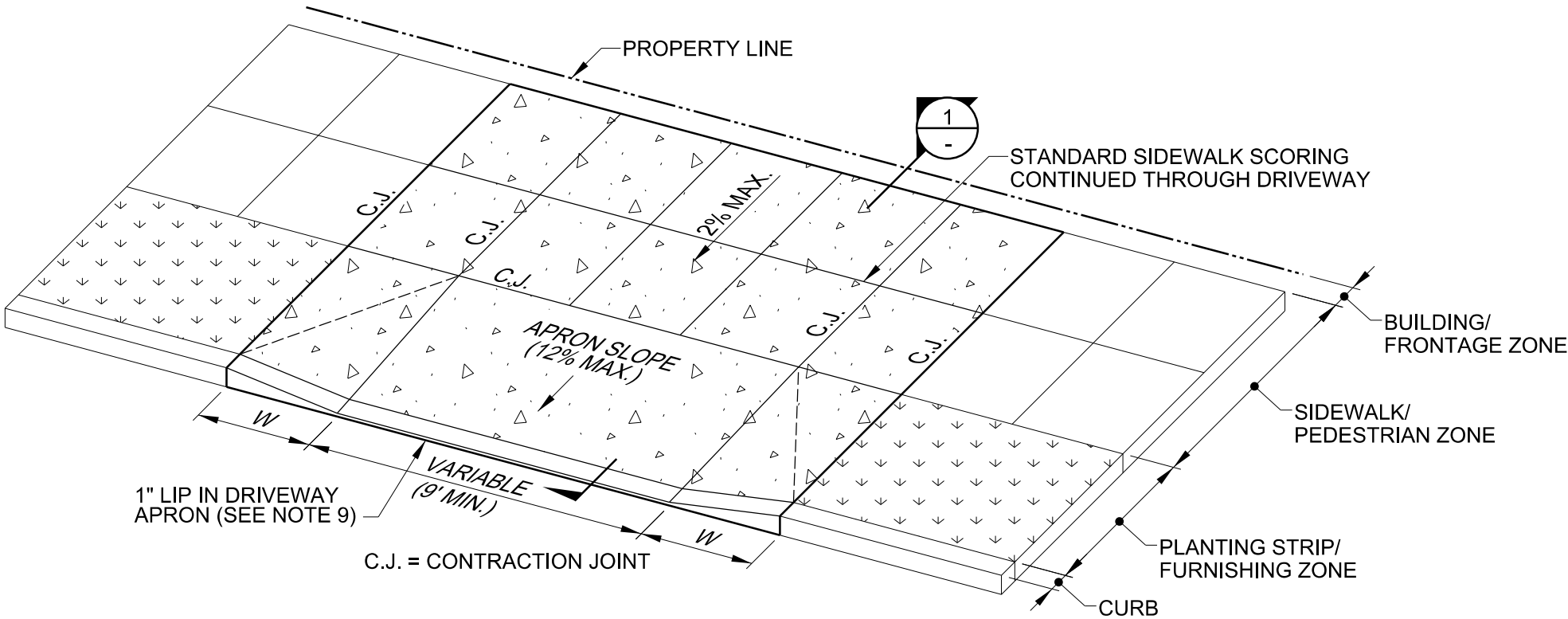
1. Cover manhole with building paper and construct asphalt concrete base course and wearing courses.
2. Saw cut square or circular excavation around manhole 12" min. from manhole frame.
3. Raise manhole cover and frame and adjust to match finish grade by installing concrete manhole adjustment rings and leveling mortar, as shown.
4. Backfill with 4000 psi. minimum Portland Cement Concrete.
5. Protect from traffic loading.
6. Apply tack coat to edges of existing pavement before installing patch.
7. Sand seal all joints.
8. See P-171 thru P-182 for manhole covers and frames.
9. Use epoxy for synthetic grade rings.
10. Do not overcut beyond what is needed to achieve full depth cut.



ALTERNATE "A"

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.			<b>PORTLAND BUREAU OF TRANSPORTATION</b>	
			 City Engineer	
			Standard Drawing Title	
			<b>MANHOLE FRAME ADJUSTMENT</b>	
Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.			Effective Date: 01-01-2017	Standard Drawing No.  <b>P-507</b>
			Calc. Book No.: -	
			Baseline Report Date: -	



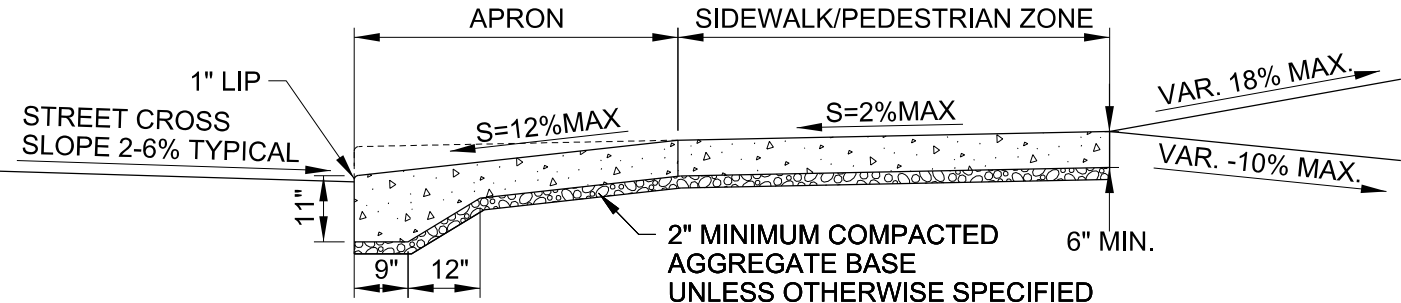


S = SLOPE  
G = ALGEBRAIC GRADE  
CHANGE AT STREET

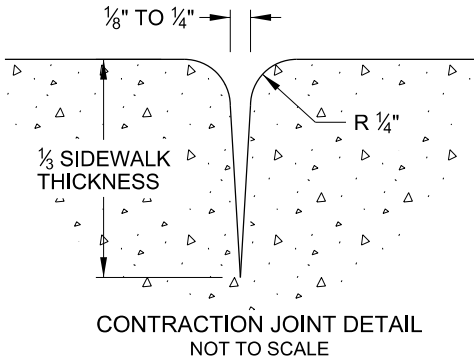
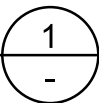
EXAMPLE CALC:  $G = 12\% - (-2\%) = 14\%$   
 $G = (\text{APRON SLOPE}) - (-\text{STREET CROSS SLOPE})$

DRIVEWAY CLASSIFICATION	WIDTH OF WINGS = W**	MAXIMUM RECOMMENDED ALGEBRAIC GRADE CHANGE AT STREET = G*	DRIVEWAY THICKNESS
RESIDENTIAL	3' MIN.	17%	6"
LIGHT COMMERCIAL/RESIDENTIAL USE	6'	12%	6"
COMMERCIAL	9'	12%	6"
HEAVY COMMERCIAL/INDUSTRIAL	12'	12%	8"

\*EVALUATE STREET CLASSIFICATION AND USE FOR APPROPRIATE GRADES  
\*\*WIDTH OF WINGS MAY BE MODIFIED SLIGHTLY TO MATCH ADJACENT SIDEWALK SCORING; COORDINATE W/ ENGINEER AND/OR INSPECTOR.



SECTION



NOTES:

- WHERE DRIVEWAY CONNECTION TO PROPERTY IS A NEGATIVE GRADE, THE GUTTER FLOW DEPTH MEASURED FROM GUTTER LINE TO BACK OF SIDEWALK ELEVATION SHALL HAVE A MINIMUM OF 3" VERTICAL RISE TO ASSURE THAT STORMWATER WILL NOT OVERTOP THE BACK OF SIDEWALK. IF THIS CONDITION CANNOT BE MET, PLACE AN INLET AT UPSTREAM SIDE OF DRIVEWAY OR OTHER APPROVED DESIGN MITIGATION.
- STANDARD SIDEWALK SCORING SHALL CONTINUE THROUGH DRIVEWAY TO DELINEATE SIDEWALK/PEDESTRIAN ZONE PER DRG. NO. P-551.
- A MINIMUM OF 3' DRIVEWAY CONNECTION SHALL BE PAVED BEHIND THE SIDEWALK TO PREVENT TRACKING OF GRAVEL OR DIRT ONTO THE SIDEWALK PER DRG. NO. P-536.
- PAY LIMIT FOR DRIVEWAYS IS THE OUTER LIMIT OF THICKER STRUCTURAL SECTION THAT IS SUBJECT TO VEHICLE LOADS.
- DRIVEWAYS SHALL BE POURED MONOLITHIC WITH CURB.
- IF APPROVED, WHERE EXISTING DRIVEWAY IS IN GOOD CONDITION AND MEETS SLOPE REQUIREMENTS, MAY CONSTRUCT ONLY AS REQUIRED FOR SATISFACTORY CONNECTION WITH NEW WORK.
- DRIVEWAY APRON SLOPE SHALL BE MAXIMIZED WHILE STILL ALLOWING THE DRIVEWAY TO FUNCTION PROPERLY BEFORE DEPRESSING THE SIDEWALK/PEDESTRIAN ZONE AS SHOWN IN THE ALTERNATIVE DEPRESSED SEPARATED SIDEWALK DRIVEWAY, DRG. NO. P-529.
- WHEN COMBINATION CURB AND GUTTER IS USED, THE GUTTER PAN SHALL BE INCLUDED IN THE PAY LIMITS FOR THE DRIVEWAY.
- WHEN EXISTING CURB EXPOSURE IS 5" OR LESS, OMIT THE 1" LIP IN DRIVEWAY APRON.
- NO VERTICAL IMPEDIMENT (POLES, SIGNS, TREES, ETC) WITHIN 3' OF THROAT.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.

Note:  
All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.

**PBOT** PORTLAND BUREAU OF TRANSPORTATION

DocuSigned by:

Steve Townsen

City Engineer

Standard Drawing Title

TYPICAL SEPARATED  
SIDEWALK DRIVEWAY

Effective Date: 02/08/2017

Calc. Book No.:

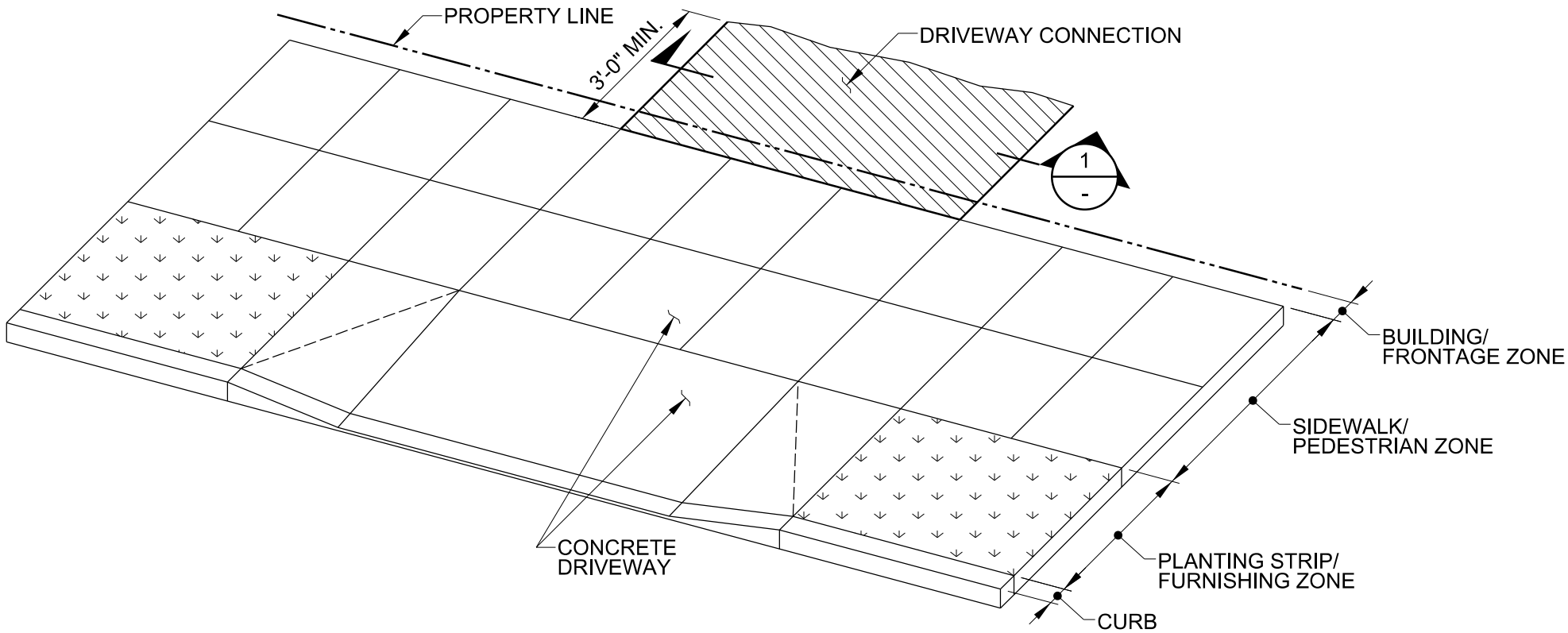
Baseline Report Date:

Standard Drawing No.

P-528

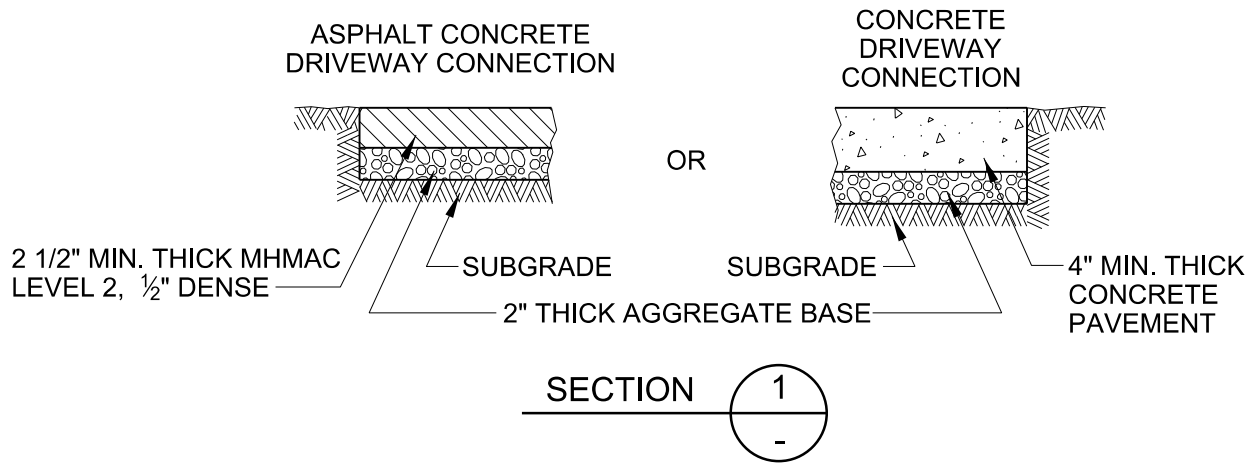


SCALE: 1" = 1'-0"



NOTES:

1. ALL DRIVEWAYS ARE REQUIRED TO HAVE A MINIMUM 3' PAVED DRIVEWAY CONNECTION BEHIND THE CONCRETE DRIVEWAY, TO PREVENT TRACKING OF DIRT AND/OR AGGREGATE ONTO SIDEWALK
2. CONSTRUCT THE DRIVEWAY CONNECTION ACCORDING TO SECTION 1.
3. DRIVEWAY CONNECTIONS MUST MEET P&Z CODES.



The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.

Note:  
All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.

**PBOT** PORTLAND BUREAU OF TRANSPORTATION

DocuSigned by:

Steve Townsend

City Engineer

Standard Drawing Title

DRIVEWAY CONNECTION

Effective Date: 02/08/2017

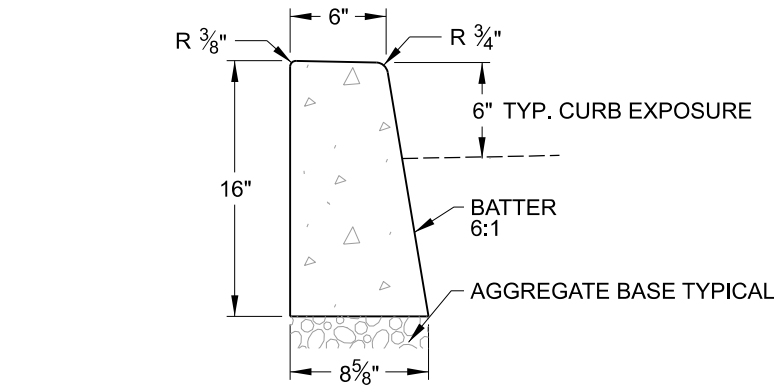
Calc. Book No.:

Baseline Report Date:

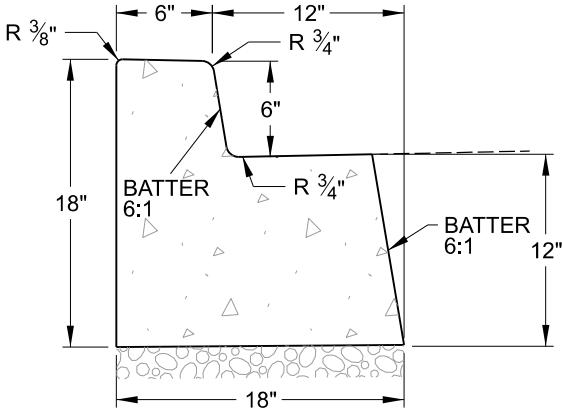
Standard Drawing No.

P-536

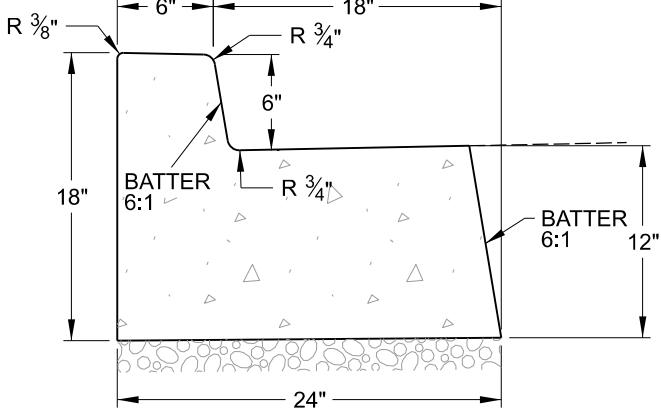




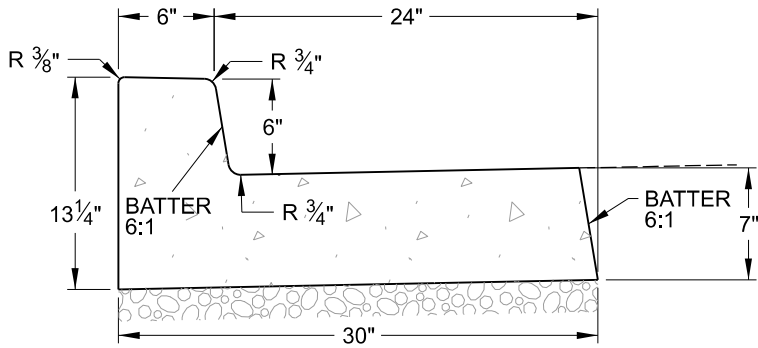
STANDARD CURB



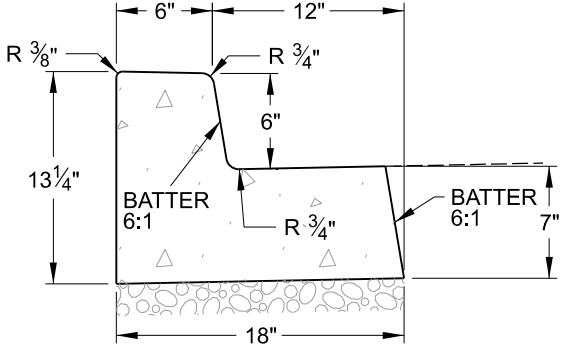
12" THICKENED CURB AND GUTTER



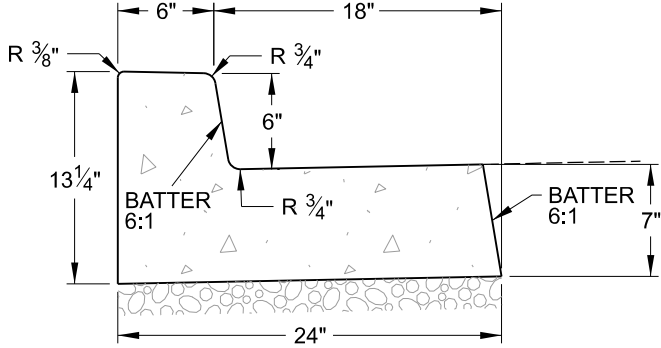
18" THICKENED CURB AND GUTTER



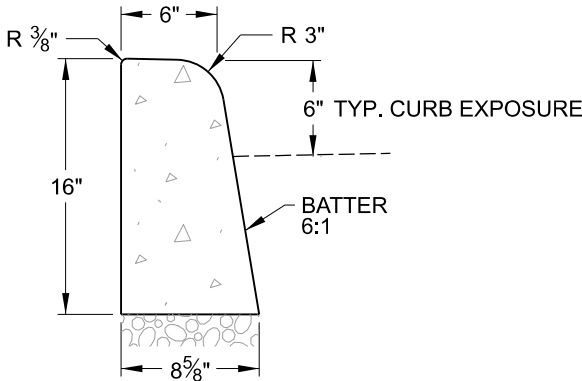
24" CURB AND GUTTER



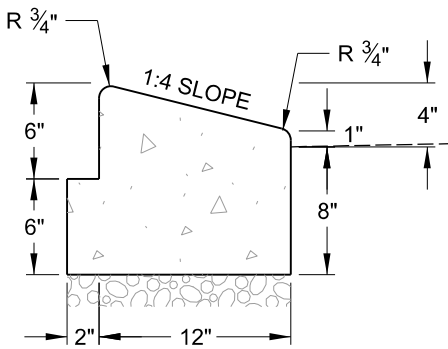
12" CURB AND GUTTER



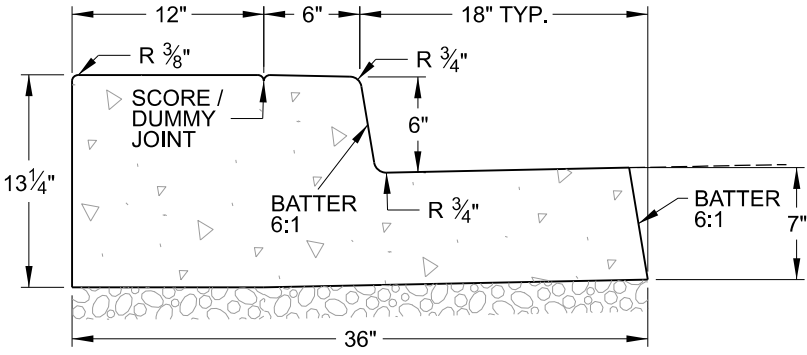
18" CURB AND GUTTER



STANDARD CURB  
MEDIAN ISLAND




LOW PROFILE  
MOUNTABLE CURB



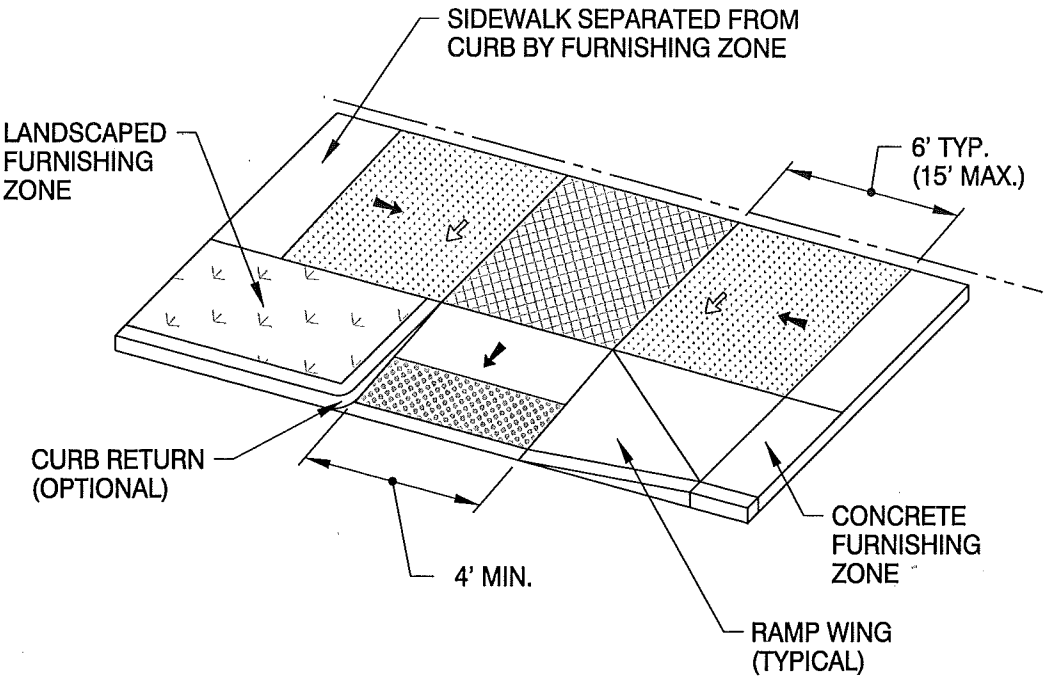
MODIFIED CURB AND GUTTER

GENERAL NOTES:

1. CURB EXPOSURE IS 6". VARY STANDARD CURB ONLY AS APPROVED.
2. WHEN MATCHING INTO EXISTING CURB WITH LESS THAN 6-INCHES OF CURB EXPOSURE, TRANSITION AT A RATE OF 1' HORIZONTAL FOR EACH 1/2" OF VERTICAL CURB DIFFERENCE. TRANSITIONS SHALL BE DONE USING STANDARD CURB.
3. CONSTRUCT CONTRACTION JOINTS AT 15' MAXIMUM SPACING AND AT ENDS OF EACH INLET AND RAMP.
4. TOPS OF ALL CURBS SHALL SLOPE TOWARD THE ROADWAY AT 2% NORMAL.
5. GUTTER PAN SHALL SLOPE BETWEEN 2% AND 6% TO MATCH CROSS SLOPE OF STREET.
6. USE 12" GUTTER PAN WHEN USING CURB AND GUTTER ADJACENT TO A BIKE LANE.
7. 2 - INCHES OF AGGREGATE BASE UNDER CURB UNLESS OTHERWISE SPECIFIED.

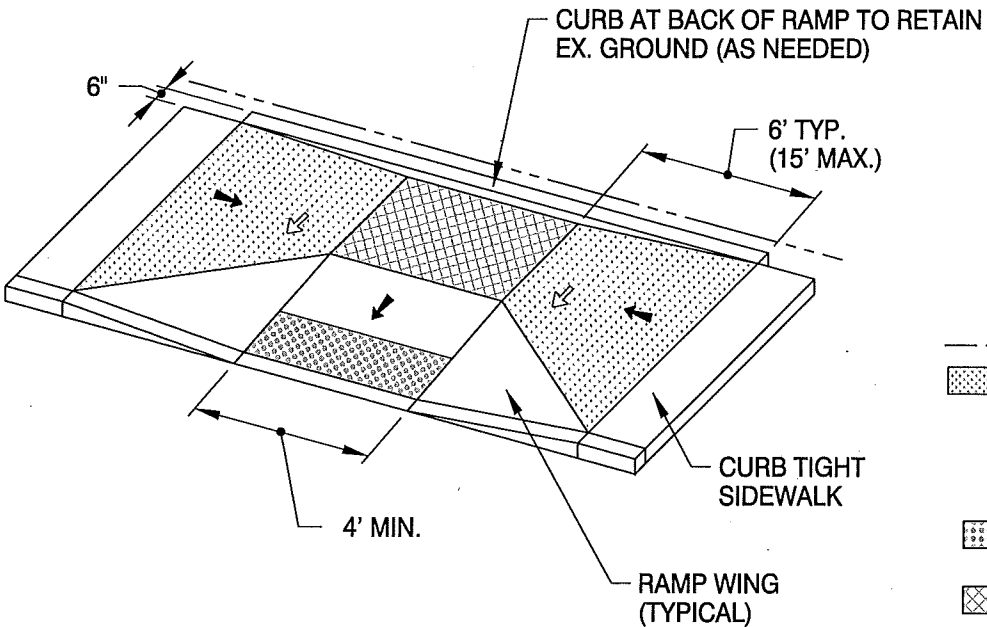
The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.	<b>PBOT</b> PORTLAND BUREAU OF TRANSPORTATION	
	 City Engineer	
	Standard Drawing Title	
	CURBS	
Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.	Effective Date: 10-04-17	Standard Drawing No.  <b>P-540</b>
	Calc. Book No.:	
	Baseline Report Date:	





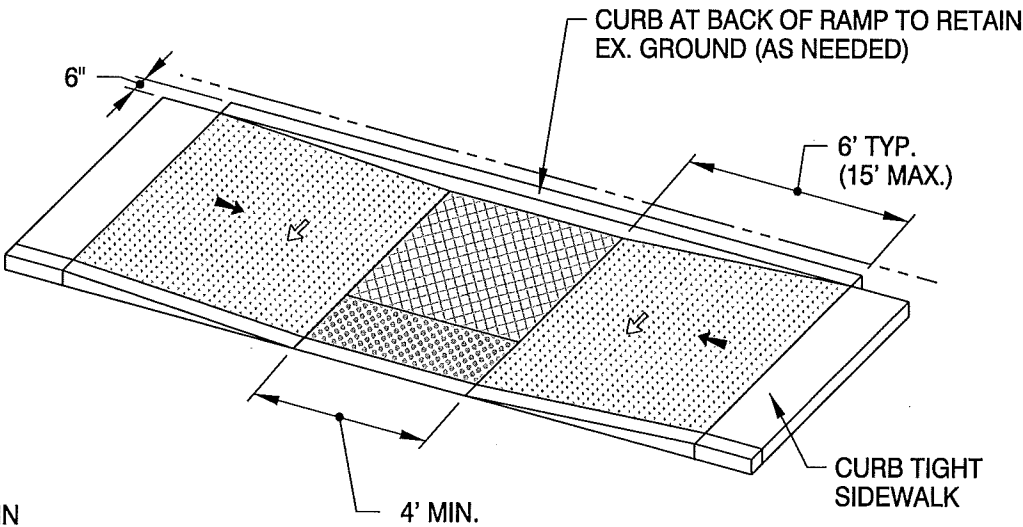
SIDEWALK RAMP THROUGH  
FURNISHING ZONE DETAIL

A



CURB TIGHT SIDEWALK - SPLIT RAMP DETAIL

B

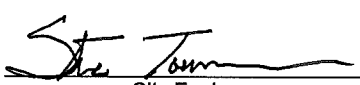


CURB TIGHT SIDEWALK - DROP RAMP DETAIL

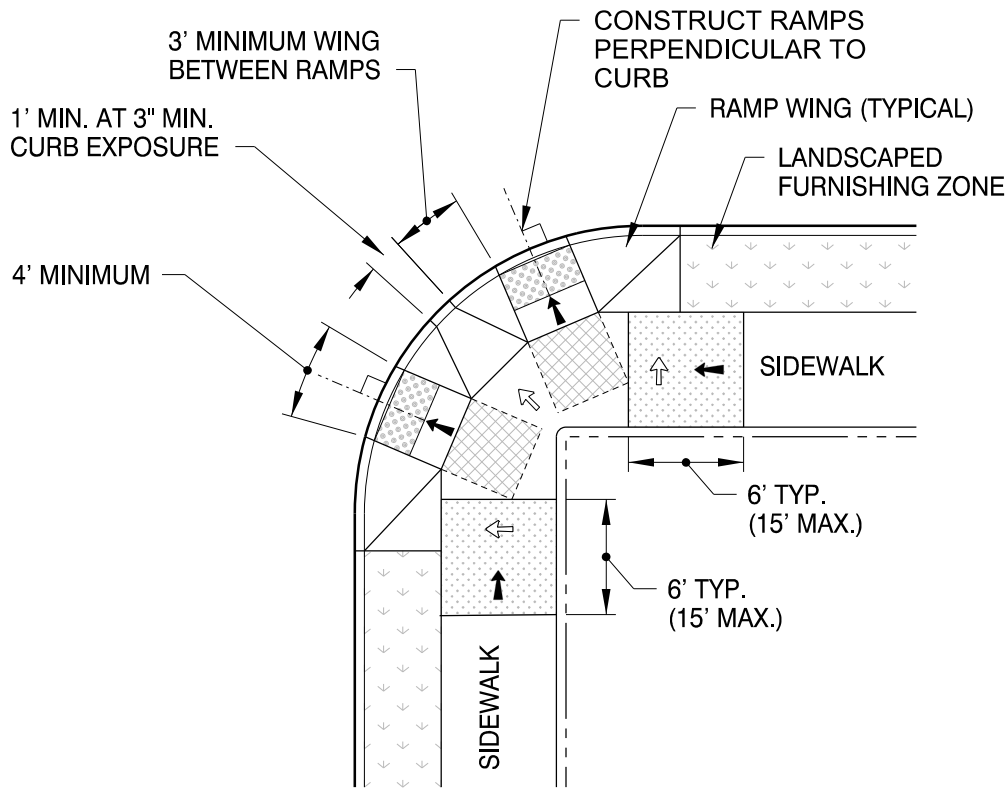
C

- PROPERTY LINE (TYPICAL)
- TRANSITION PANEL TO RAMP LANDING  
SLOPE 2% MAX.
- SLOPE 8.33% MAX. (USE 7.2% FOR DESIGN  
GRADE MAXIMUM FOR CONSTRUCTION TOLERANCE)
- TRUNCATED DOME DETECTABLE WARNING SURFACE  
(SEE NOTE #5 AND ODOT STD. DRG. NO. RD759 FOR DETAILS)
- LANDING AREA (MINIMUM LEVEL AREA 48" X 48")  
FOR THE PURPOSES OF THIS APPLICATION, A 2%  
MAXIMUM SLOPE IS CONSIDERED LEVEL.  
(USE 1.5% FOR DESIGN GRADE MAXIMUM FOR CONSTRUCTION  
TOLERANCE.)

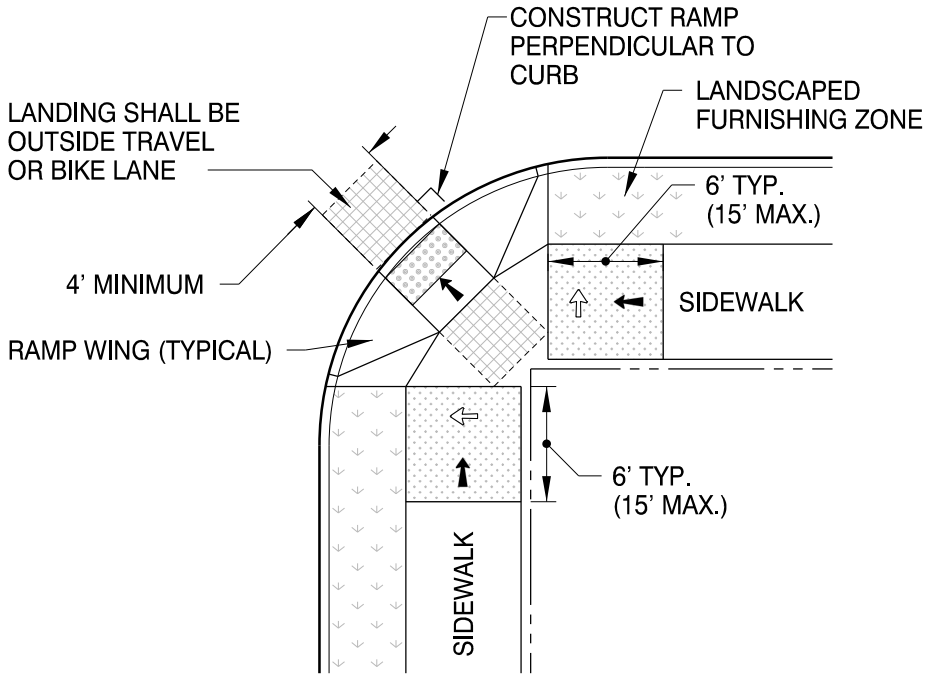
- NOTES:**
- SIDEWALK RAMP DETAILS ARE BASED ON US DOT REQUIREMENTS.
  - SEE STD. DRG. NO. P-540 FOR CURBS AND P-551 FOR SIDEWALKS.
  - TOOLED JOINTS ARE REQUIRED AT ALL SIDEWALK RAMP SLOPE BREAK LINES.
  - SIDEWALK CURB RAMP SLOPES SHOWN ARE RELATIVE TO THE TRUE LEVEL HORIZON (ZERO BUBBLE).
  - PLACE TRUNCATED DOME DETECTABLE WARNING SURFACE 6" FROM FACE OF CURB ADJACENT TO TRAFFIC. THE WIDTH OF THE DETECTABLE WARNING SHALL BE THE SAME WIDTH AS THE RAMP THROAT. DETECTABLE WARNINGS SHALL BE FEDERAL YELLOW. SEE ODOT STD. DRG. NO. RD759 FOR DETAILS NOT SHOWN.
  - RAMP WING FLARES MAY BE 10% MAX. SLOPE RELATIVE TO STREET GRADE.
  - CHECK THE GUTTER FLOW DEPTH TO ASSURE THAT THE DESIGN FLOOD DOES NOT OVERTOP THE BACK OF SIDEWALK. PLACE INLET AT UPSTREAM SIDE OR PERFORM OTHER APPROVED DESIGN MITIGATION IF BACK OF RAMP LANDING IS LESS THAN 3" ABOVE GUTTER LINE.
  - A CURB RETURN INSTEAD OF RAMP WING MAY ONLY BE USED WHEN THE RAMP IS PROTECTED FROM PEDESTRIAN CROSS-TRAVEL (BY HANDRAIL OR SIMILAR ACCEPTABLE BARRIER) OR WHEN THE ADJACENT FURNISHING ZONE IS LANDSCAPED.
  - RAMPS MUST BE ADA COMPLIANT. CONSTRUCT ALL RAMPS PERPENDICULAR TO CURB.
  - CURB RAMP AT STREET INTERFACE SHALL BE WITHIN LEGAL CROSSWALK AND HAVE A MAXIMUM ALGEBRAIC GRADE BREAK OF 11% (STREET CROSS SLOPE TO RAMP GRADE).
  - MIDBLOCK RAMPS SHOULD MATCH THE WIDTH OF CORRESPONDING SIDEWALK, UP TO 6 FEET.
  - PROVIDE MINIMUM 1 FOOT CLEAR (FREE FROM VERTICAL OBSTRUCTIONS, INCLUDING CURBS) AT BACK OF LANDINGS.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.	PORTLAND BUREAU OF TRANSPORTATION	
	PBOT  City Engineer	
	Standard Drawing Title	
	MIDBLOCK SIDEWALK RAMP	
Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.	Effective Date: 04-03-2017	Standard Drawing No.  P-547
	Calc. Book No.:	
	Baseline Report Date:	

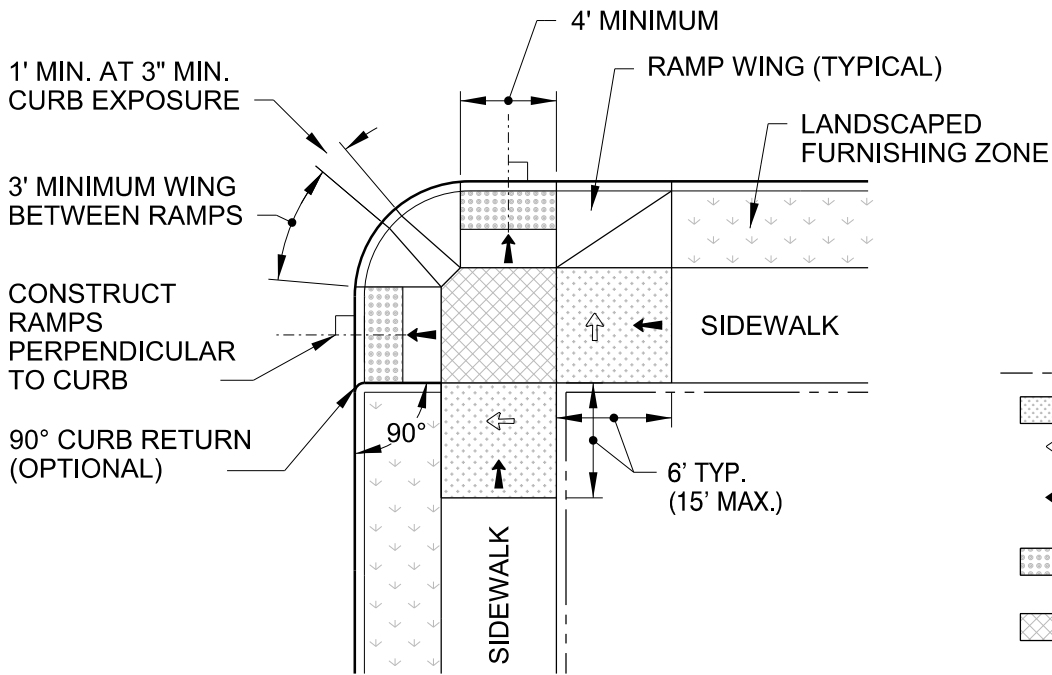




DUAL RAMPS WITH PLANTER STRIP (LARGER RADIUS - 15' AS SHOWN) A



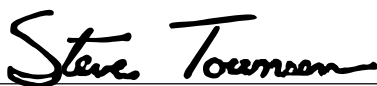
SINGLE RAMP WITH PLANTER STRIP (FOR USE WHEN DUAL RAMPS CANNOT BE ACCOMMODATED) C



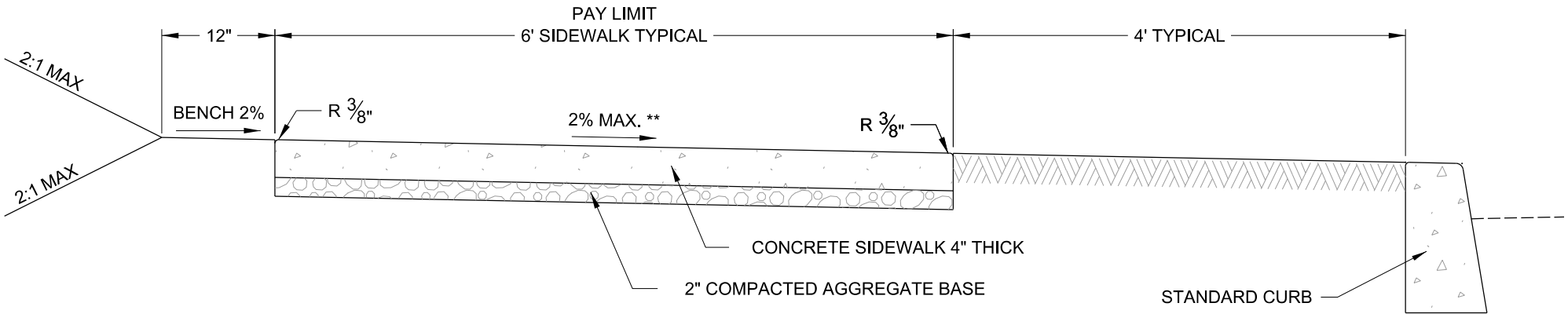
DUAL RAMPS WITH PLANTER STRIP (SMALLER RADIUS - 6' AS SHOWN) B

- PROPERTY LINE (TYPICAL)
- [Pattern] TRANSITION PANEL TO RAMP LANDING SLOPE 2% MAX.
- [Pattern] SLOPE 8.33% MAX. (USE 7.2% FOR DESIGN GRADE MAXIMUM FOR CONSTRUCTION TOLERANCE)
- [Pattern] TRUNCATED DOME DETECTABLE WARNING SURFACE (SEE NOTE #5 AND ODOT STD. DRG. NO. RD759 FOR DETAILS)
- [Pattern] LANDING AREA (MINIMUM LEVEL AREA 48" X 48") FOR THE PURPOSES OF THIS APPLICATION, A 2% MAXIMUM SLOPE IS CONSIDERED LEVEL. (USE 1.5% FOR DESIGN GRADE MAXIMUM FOR CONSTRUCTION TOLERANCE.)

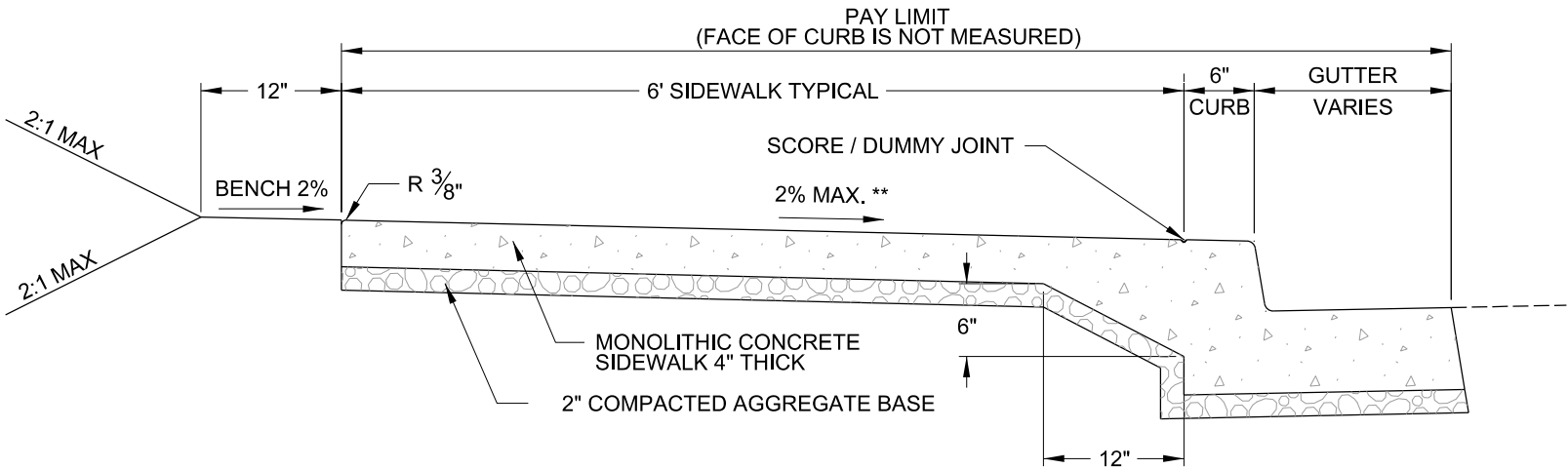
- NOTES:
- SIDEWALK RAMP DETAILS ARE BASED ON US DOT REQUIREMENTS.
  - SEE STD. DRG. NO. P-540 FOR CURBS AND P-551 FOR SIDEWALKS.
  - TOOLED JOINTS ARE REQUIRED AT ALL SIDEWALK RAMP SLOPE BREAK LINES.
  - SIDEWALK CURB RAMP SLOPES SHOWN ARE RELATIVE TO THE TRUE LEVEL HORIZON (ZERO BUBBLE).
  - PLACE TRUNCATED DOME DETECTABLE WARNING SURFACE 6" FROM FACE OF CURB ADJACENT TO TRAFFIC. THE WIDTH OF THE DETECTABLE WARNING SHALL BE THE SAME WIDTH AS THE RAMP THROAT. DETECTABLE WARNINGS SHALL BE FEDERAL YELLOW. SEE ODOT STD. DRG. NO. RD759 FOR DETAILS NOT SHOWN.
  - RAMP WING FLARES MAY BE 10% MAX. SLOPE RELATIVE TO STREET GRADE.
  - CHECK THE GUTTER FLOW DEPTH TO ASSURE THAT THE DESIGN FLOOD DOES NOT OVERTOP THE BACK OF SIDEWALK. PLACE INLET AT UPSTREAM SIDE OR PERFORM OTHER APPROVED DESIGN MITIGATION IF BACK OF RAMP LANDING IS LESS THAN 3" ABOVE GUTTER LINE.
  - A CURB RETURN INSTEAD OF RAMP WING MAY ONLY BE USED WHEN THE RAMP IS PROTECTED FROM PEDESTRIAN CROSS-TRAVEL (BY HANDRAIL OR SIMILAR ACCEPTABLE BARRIER) OR WHEN THE ADJACENT FURNISHING ZONE IS LANDSCAPED.
  - RAMPS MUST BE ADA COMPLIANT. CONSTRUCT ALL RAMPS PERPENDICULAR TO CURB MEASURED AT RAMP CENTERLINE.
  - CURB RAMP AT STREET INTERFACE SHALL BE WITHIN LEGAL CROSSWALK AND HAVE A MAXIMUM ALGEBRAIC GRADE BREAK OF 11% (STREET CROSS SLOPE TO RAMP GRADE).
  - MAINTAIN EXISTING CURB RETURN/CORNER RADIUS FOR RETROFITS UNLESS OTHERWISE DIRECTED.
  - PROVIDE MINIMUM 1 FOOT CLEAR (FREE FROM VERTICAL OBSTRUCTIONS, INCLUDING CURBS) AT BACK OF LANDINGS.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.	<b>PBOT</b> PORTLAND BUREAU OF TRANSPORTATION  City Engineer	
	Standard Drawing Title  SIDEWALK RAMP WITH PLANTER STRIP PLACEMENT OPTIONS	
	Effective Date: 03-22-2018	Standard Drawing No.  P-548
	Calc. Book No.: n/a	
Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.	Std. Drg. Report Date: 03-22-2018	

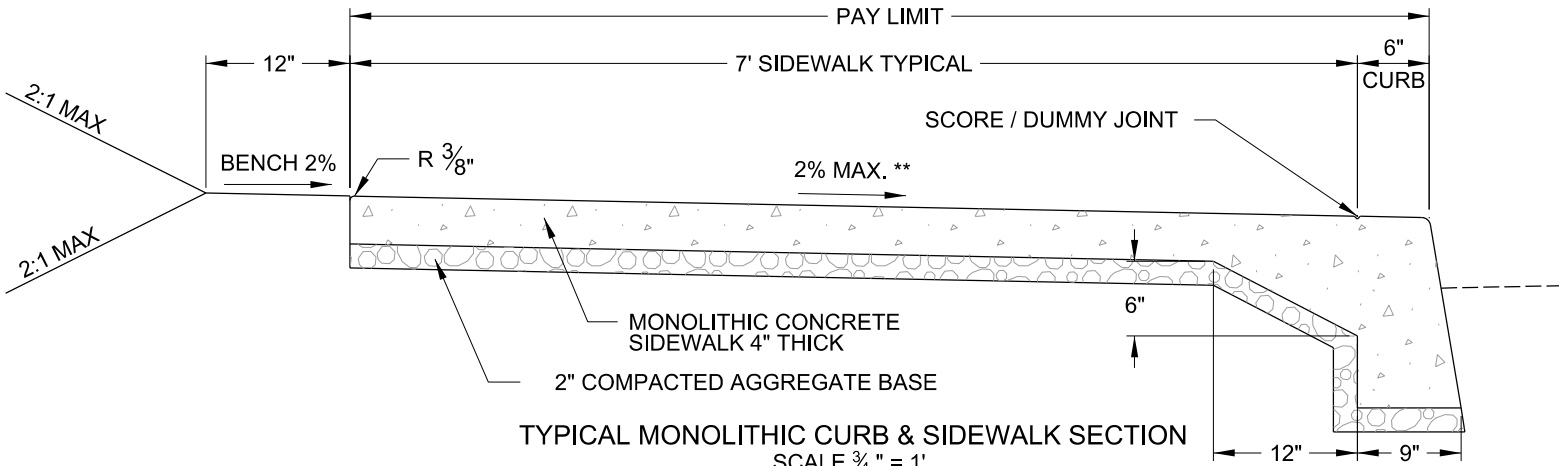




TYPICAL SEPARATED SIDEWALK SECTION  
SCALE 3/4" = 1'



TYPICAL MONOLITHIC CURB GUTTER & SIDEWALK SECTION  
SCALE 3/4" = 1'



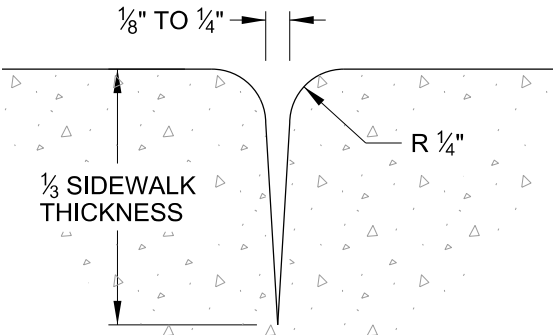
TYPICAL MONOLITHIC CURB & SIDEWALK SECTION  
SCALE 3/4" = 1'

GENERAL NOTES:

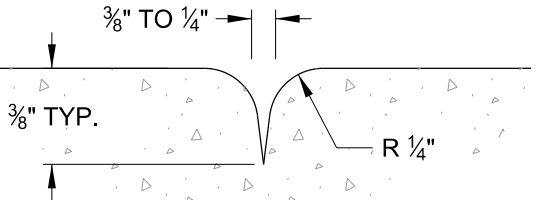
1. FOR CURB DETAILS SEE DRAWING P-540. ALL CURB FACE TO HAVE A 6:1 BATTER.
2. CONSTRUCT CONTRACTION JOINTS AT 15' MAXIMUM SPACING, AND AT ENDS OF EACH DRIVEWAY RAMP.
3. CONSTRUCT EXPANSION JOINTS ADJACENT TO BUILDINGS AND AT WOOD POLES.
4. NO SEPARATE PAYMENT MADE FOR FACE OF CURB UNLESS SPECIFIED.
5. PROVIDE LIGHT BROOM FINISH TRANSVERSE TO THE LINE OF TRAFFIC.
6. "SHINERS" OR SMOOTH TROWELLED JOINTS SHALL NOT BE USED UNLESS MATCHING INTO EXISTING CONDITIONS.
7. MATCH SCORING PATTERN WHEN ABUTTING EXISTING SIDEWALK UNLESS OTHERWISE DIRECTED.
8. SAWCUT CONTRACTION JOINTS WHEN SIDEWALK IS A DESIGNATED OR SHARED BICYCLE PATH. SEE PLANS FOR SAWCUT LAYOUT AND DETAILS

SIDEWALK SCORING *	
ZONE WIDTH	SCORING GRID
5'	5' x 5'
6'	3' x 3'
7'	3.5' x 3.5'
8'	4' x 4'
10'	5' x 5'
12'	4' x 4'
15'	5' x 5'

\* MODIFY SCORING AT CORNERS TO MEET ADA REQUIREMENTS.



CONTRACTION JOINT DETAIL  
NOT TO SCALE



SCORE / DUMMY JOINT DETAIL  
NOT TO SCALE

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.

Note:  
All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.

**PBOT** PORTLAND BUREAU OF TRANSPORTATION

*Steve Tamm*  
City Engineer

Standard Drawing Title

**SIDEWALKS**

Effective Date: 07-05-2017

Calc. Book No.:

Baseline Report Date:

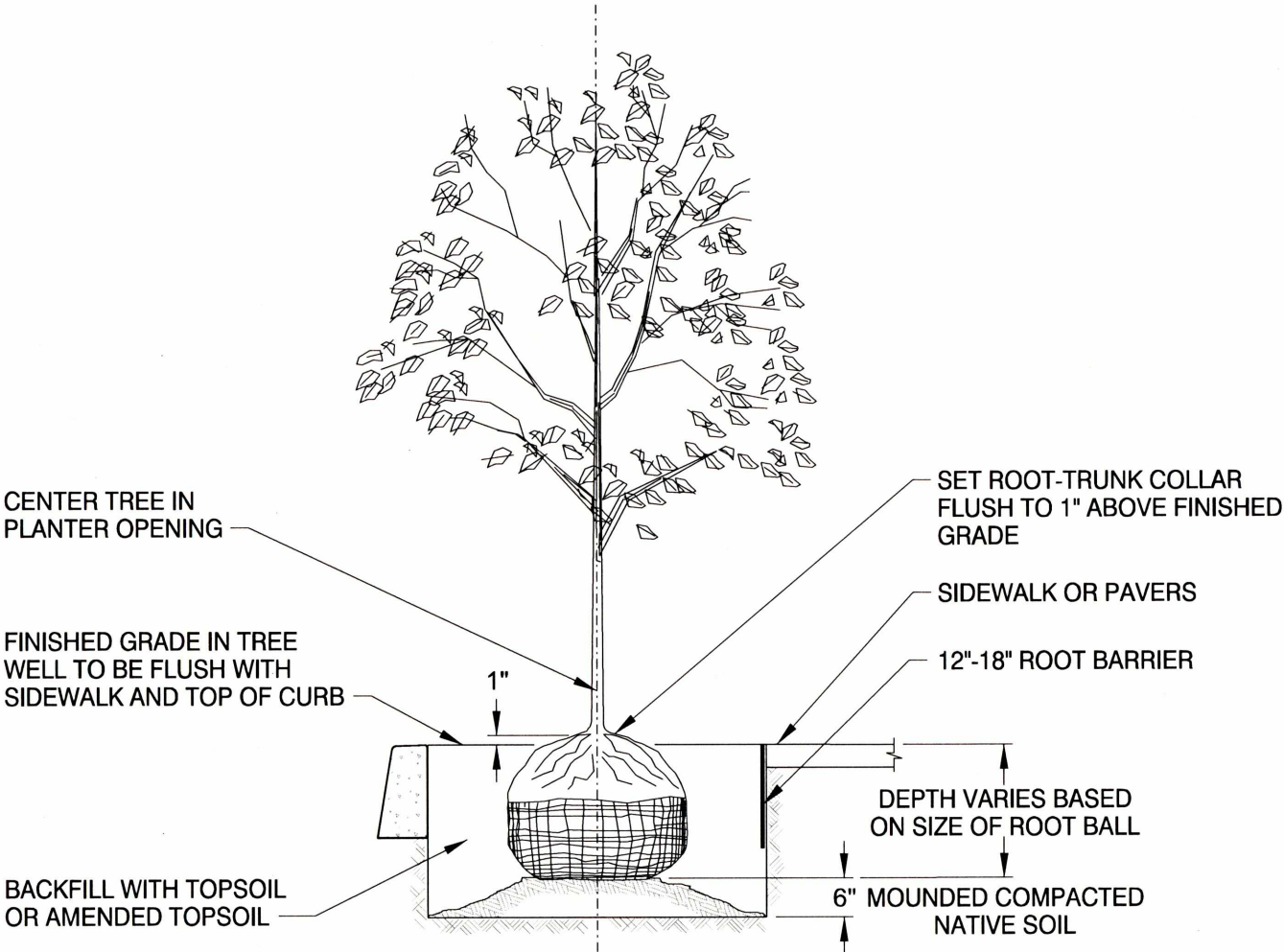
Standard Drawing No.

**P-551**

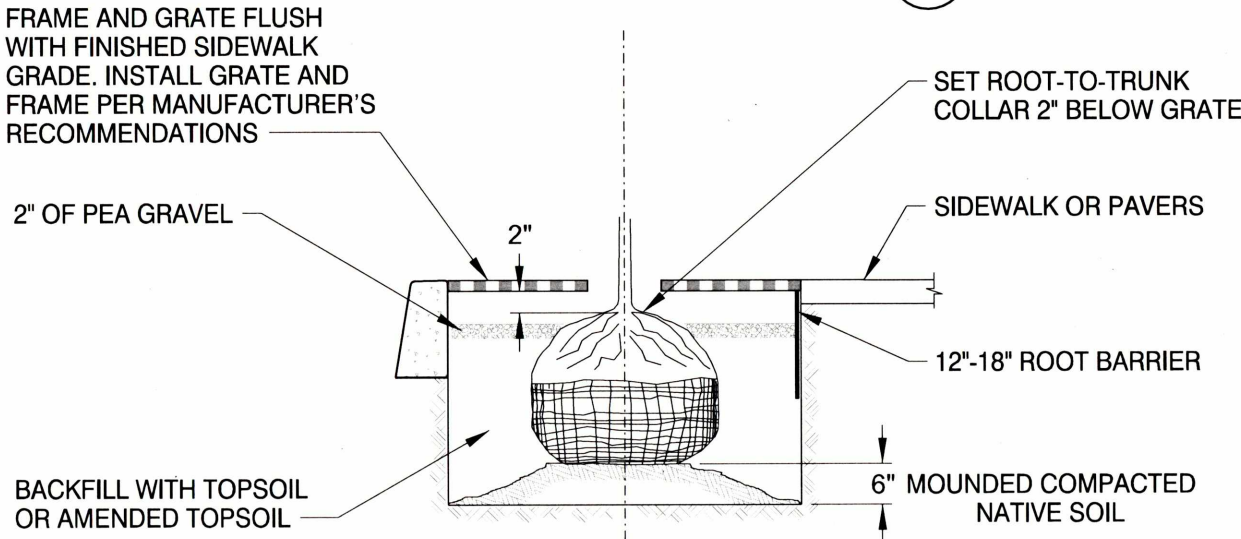
\*\* USE 1.5% FOR DESIGN GRADE MAXIMUM FOR CONSTRUCTION TOLERANCE.



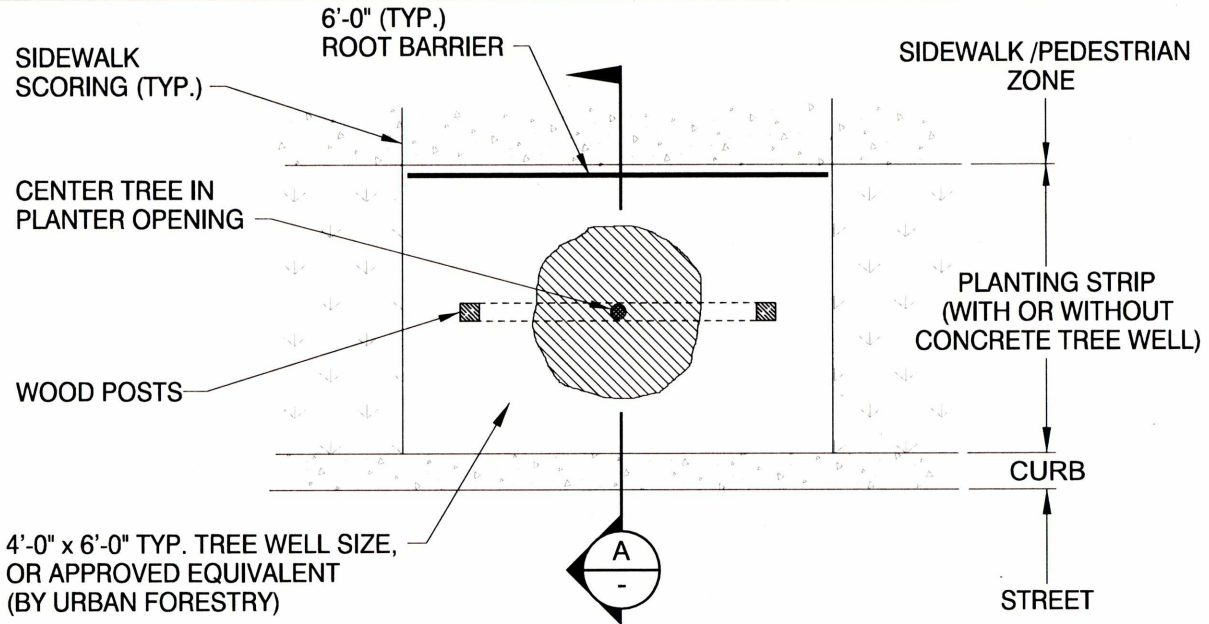
Plot Date: 4/20/2012 2:24:08 PM Filename: U:\Illustration\CITY OF PORTLAND - STANDARD DRAWINGS\SECTION DIVISIONS\3-STREET\STREET\WICROSTATION\STD DRAWING NO P-58\A\A\T-SHT\_P-58.dgn



TYPICAL STREET TREE SECTION 1 A


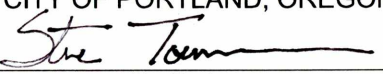


TYPICAL STREET TREE WITH GRATE SECTION 2 A



PLAN VIEW

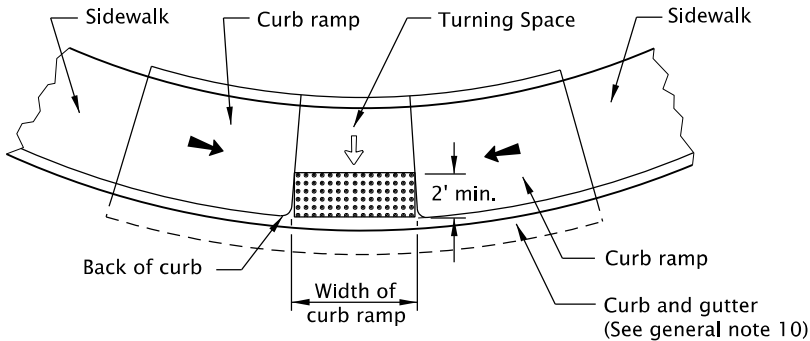
- NOTES:
1. EXCAVATE 2½ TIMES THE SIZE OF ROOT BALL OR TREE WELL DIMENSIONS.
  2. CAREFULLY REMOVE AS MUCH OF BURLAP AND BASKET AS POSSIBLE WITHOUT CAUSING DAMAGE TO ROOTBALL.
  3. TREE GRATES ARE NOT PREFERRED AND SUBJECT TO APPROVAL. IF USED TREE GRATES MUST BE ADA COMPLIANT. USE PRODUCTS ON CPL OR AS APPROVED BY URBAN FORESTRY.
  4. WOOD POSTS WITH APPROVED TIE STRAPS MAY BE NECESSARY FOR TREE STABILITY. INSTALL PARALLEL TO CURB.
  5. WHEN PLANTING NEAR STREET LIGHTING, SIGNAGE AND OTHER UNDERGROUND UTILITIES, REFER TO URBAN FORESTRY'S "STREET TREE PLANTING AND ESTABLISHMENT GUIDELINES". CONTACT URBAN FORESTRY @ (503) 823-4489 FOR FURTHER INFORMATION.
  6. ROOT BARRIER SHALL BE USED ALONG SIDEWALK /PEDESTRIAN ZONE SIDE OF STREET TREE. USE PRODUCTS ON CPL OR AS APPROVED BY URBAN FORESTRY.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.	 <div>BUREAU OF TRANSPORTATION CITY OF PORTLAND, OREGON</div> <div> City Engineer</div>	
	Standard Drawing Title	
Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.	TYPICAL STREET TREE INSTALLATION	
	Effective Date: 04/20/2012	Standard Drawing No.  P-581
	Calc. Book No.:	
	Baseline Report Date:	

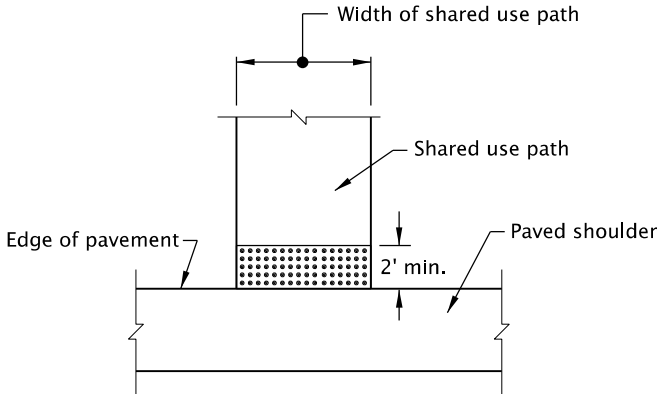


rd759.dgn 16-JAN-2019

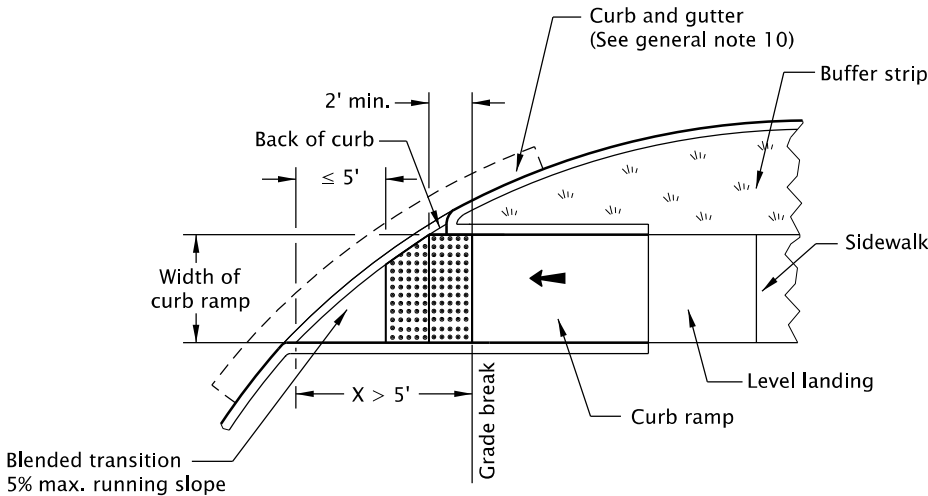
RD759



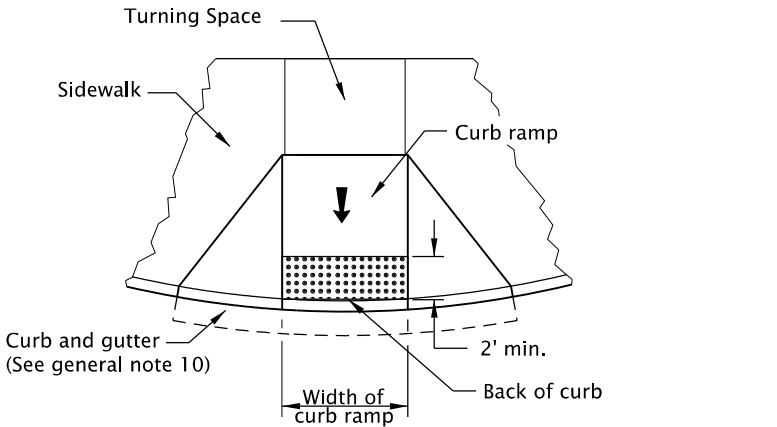
PARALLEL CURB RAMP



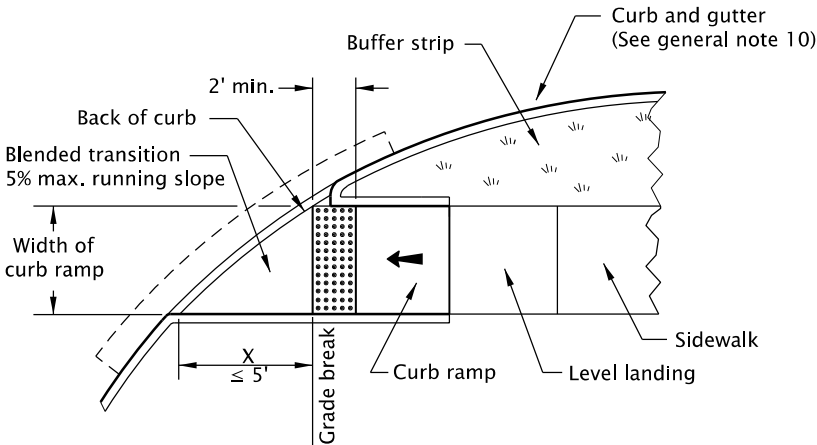
SHARED-USE PATH CONNECTION



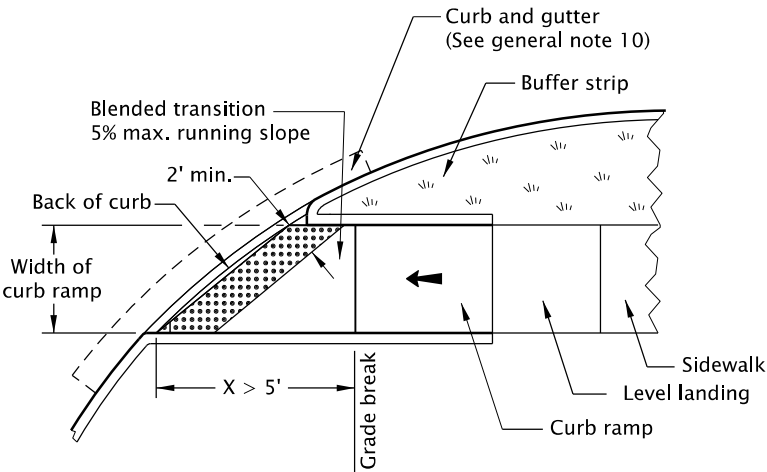
CURB RAMP CROSSING  
GRADE BREAK > 5 FT. FROM BACK OF CURB  
OPTION 1  
(Detectable warning surface shall be placed in area from curb ramp grade break to within 5' of curb)



PERPENDICULAR CURB RAMP



CURB RAMP CROSSING  
GRADE BREAK ≤ 5 FT. FROM BACK OF CURB  
(Detectable warning surface shall be placed on the bottom of the curb ramp directly above the grade break)



CURB RAMP CROSSING  
GRADE BREAK > 5 FT. FROM BACK OF CURB  
OPTION 2  
(Detectable warning surface shall be placed in the lower 2' of curb ramp that is adjacent to traffic)

- GENERAL NOTES FOR ALL DETAILS ON THIS SHEET:
- Detectable warning surface details & locations are based on ODOT applicable Standards.
  - See project plans for details not shown.  
See Std. Dwgs. RD700 & RD701 for curbs.  
See Std. Dwg. RD720 for sidewalks.  
See Std. Dwgs. TM503 & TM530 for crosswalk markings, widths, etc.  
See Std. Dwgs. RD705 & RD710 for islands.
  - The Detectable Warning Surface shall extend the full width of the curb ramp, or other roadway entrance as applicable. A gap of up to 2 inches on each side of the Detectable warning surface is permitted (Measured at the leading corners of the detectable warning surface panel).
  - Detectable warning surface shall be placed at the back of curb for a minimum depth of 2 ft. at curb ramps that adjacent to traffic. Detectable warning surface may be radial or rectangular, but must comply with the truncated dome size and spacing standards. Detectable warning surface may be cut to meet necessary shape as shown in plans. Color to be safety yellow if no color specified in construction note. For detectable warning surface on or along state highway, alternative colors must be approved.
  - Detectable warning surface shall be used in the following locations:
    - Curb ramps (See Std. Dwgs. RD755, RD756, & RD757).
    - Crossing islands (Accessible Route Islands), (See Std. Dwg. RD710).
    - Rail crossings (See Std. Dwg. RD758).
  - Where public transportation stations (rail, bus, etc.) use platform boarding, detectable warning surface shall be placed along the full edge length of the station, when not protected by platform screens or guards (See Std. Dwg. RD758).
  - Detectable warning surface shall not be used on the following locations:
    - End of sidewalk transitions that are not at a crosswalk, (See Std. Dwg. RD754).
    - Driveways, unless constructed with curb return, (See Std. Dwgs. RD725, RD730, RD735, RD740, RD745, & RD750).
    - Parking lots.
  - Grade breaks at the top and bottom of curb ramp runs shall be perpendicular to the direction of the ramp run. Grade breaks shall not be permitted on the surface of ramp runs and turning spaces. Surface slopes that meet at grade breaks shall be flush.
  - Where no curb is present, the detectable warning surface shall be placed at the edge of the roadway.
  - On or along state highways, curb and gutter is required at curb ramps.
  - Detectable warning surface placement for perpendicular ramps vary as shown.

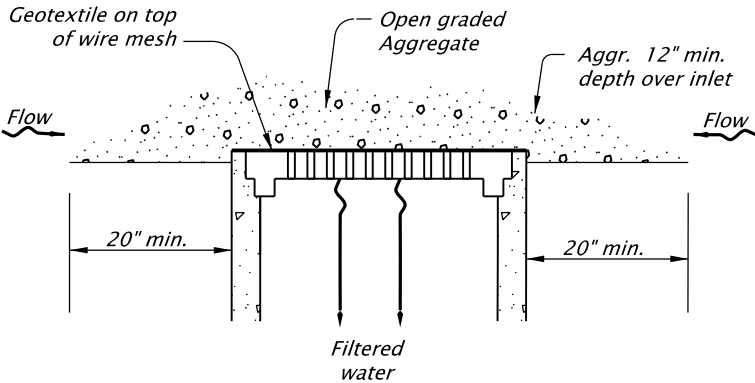
	Detectable warning surface
	Slope 1.5% max. (Max. 2.0% finished surface slope)
	Slope 7.5% max. (Max. 8.3% finished surface slope)

CALC. BOOK NO. <u>  N/A  </u>		BASELINE REPORT DATE <u>  16-JAN-2019  </u>	
<i>The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.</i>		NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications	
		<b>OREGON STANDARD DRAWINGS</b>	
		<b>DETECTABLE WARNING SURFACE DETAILS &amp; PLACEMENT LOCATIONS</b>	
		2018	
		DATE	REVISION DESCRIPTION
		07-2018	REPLACED DRAWING TITLE, REVISED DETAILS & NOTES
		09-2018	REVISED DETAIL & NOTES
		01-2019	REVISED DETAIL & NOTES

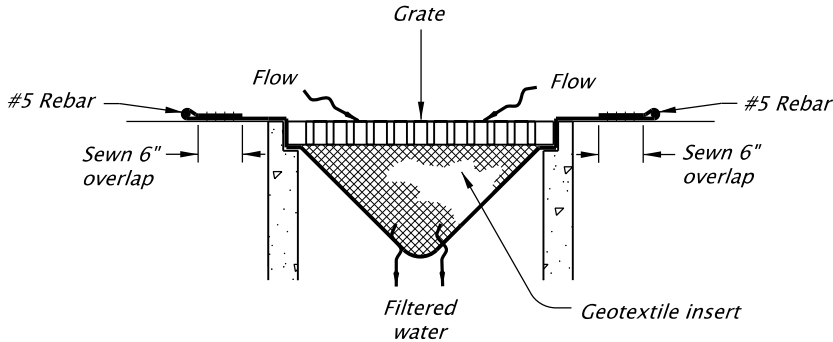


rd1010.dgn 10-01-2018

RD1010

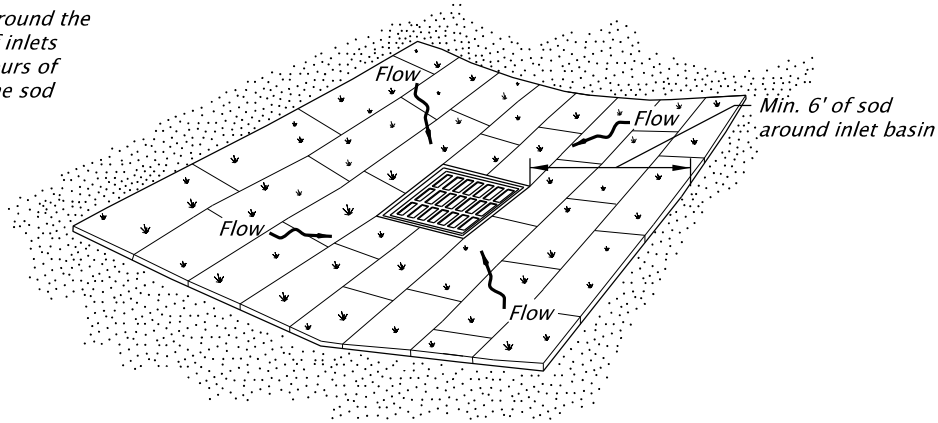


GEOTEXTILE/WIRE MESH/AGGREGATE - TYPE 2

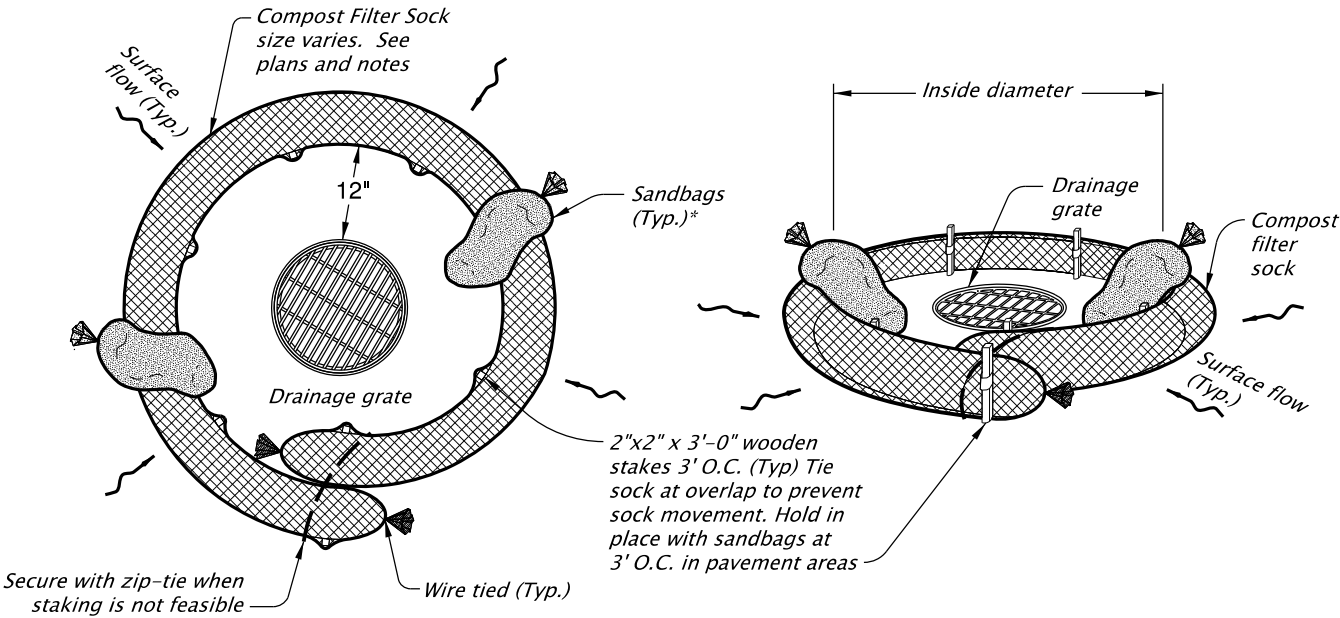


PREFABRICATED FILTER INSERT - TYPE 3

Note:  
Install sod around the  
perimeter of inlets  
within 36 hours of  
harvest of the sod

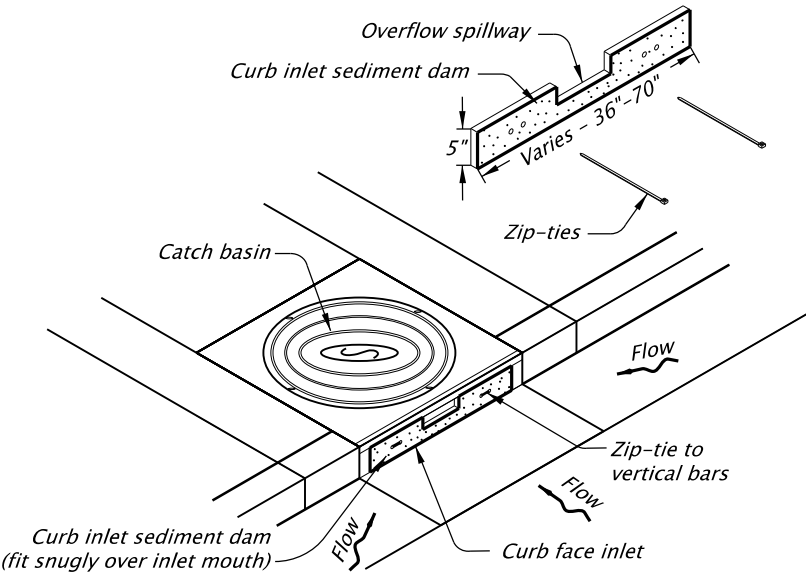


SOD PROTECTION - TYPE 6

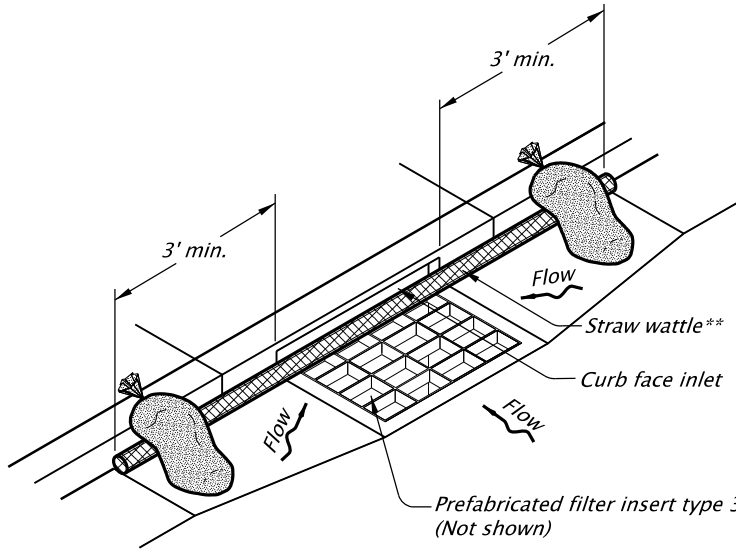


AREA DRAIN PLAN

AREA DRAIN PERSPECTIVE VIEW

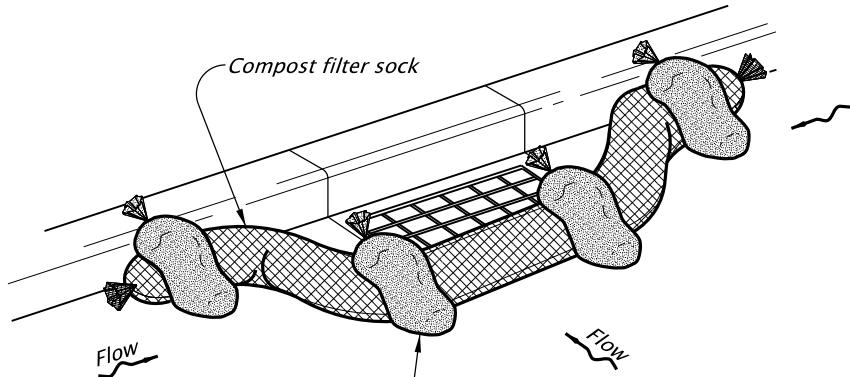


CURB INLET SEDIMENT DAM - TYPE 10



WATTLE BARRIER WITH FILTER INSERT - TYPE 11

\*\* Straw wattle drawn NTS to show curb inlet opening



CURB INLET PERSPECTIVE VIEW

COMPOST FILTER SOCK OR WATTLE - TYPE 7

\* Use sandbags to hold wattles in place. Sandbags are not necessary for compost filter socks

Notes:

Type 2 – Geotextile/wire mesh/aggregate  
Place the wire mesh over the grate.  
Place sediment fence geotextile over the wire mesh and perimeter area around structure.  
Install aggregate over the geotextile fabric.

Type 3 – Prefabricated filter inserts  
Install prefabricated filter inserts according to the plans, special provisions, and manufacturer recommendations.  
Prefabricated inserts with provisions for overflow are allowed only when accompanied by additional BMP's to prevent the potential of sediments entering project storm systems.  
Field fabricated inserts are not allowed.

Type 7 – Compost filter sock  
Drive 2" X 2" wood stakes a minimum of 6" into ground and flush with the top of the sock.  
Overlap ends of sock per manufacturers recommendations

Type 7 cont. – (1' min, 3' max).  
Use 8" to 12" dia sock on curbside in traffic areas.  
Use 12" to 18" dia sock in non-traffic areas or areas where the larger socks can be used safely.

Type 10 – Curb inlet sediment dam  
Fit curb inlet sediment dam snugly into inlet mouth. Curb inlet sediment dam is required for use with inlet filter insert where at-grade inlet grate and curb inlet are combined at a catch basin.

Type 11 – Wattle barrier with filter insert  
Install prefabricated filter insert per type 3 detail.  
Install wattles over opening and 3' to each side of opening tight against curb. Adjust wattle to force storm water to flow through filter insert or wattle prior to leaving the site. Adjust, replace or modify the inlet protection as needed to prevent sediment laden water from entering the catch basin.

CALC. BOOK NO. 6402, 6406, 6407

BASELINE REPORT DATE October 2018

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS

INLET PROTECTION  
TYPE 2, 3, 6, 7, 10 and 11

2018

DATE	REVISION	DESCRIPTION
01/2018	Added type 10 and 11	
10/2018	Corrected Sheet title to include added inlet protection details	

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tm800.dgn 01-JAN-2019

TM800

TAPER TYPES & FORMULAS	
TAPER	FORMULA
Merging (Lane Closure)	"L"
Shifting	"L"/2 or ½"L"
Shoulder Closure	"L"/3 or ⅓"L"
Flagging (See Drg. TM850)	50' – 100'
Downstream (Termination)	Varies (See Drawings)

★ Use Pre-Construction Posted Speed to select the Speed from the Tables below:

CONCRETE BARRIER FLARE RATE TABLE	
★SPEED (mph)	MINIMUM FLARE RATE
≤ 30	8:1
35	9:1
40	10:1
45	12:1
50	14:1
55	16:1
60	18:1
65	19:1
70	20:1

MINIMUM LENGTHS TABLE					
"L" VALUE FOR TAPERS (ft)					BUFFER "B" (ft)
★ SPEED (mph)	W = Lane or Shoulder Width being closed or shifted				
	W ≤ 10	W = 12	W = 14	W = 16	
25	105	125	145	165	75
30	150	180	210	240	100
35	205	245	285	325	125
40	265	320	375	430	150
45	450	540	630	720	180
50	500	600	700	800	210
55	550	660	770	880	250
60	600	720	840	960	285
65	650	780	910	1000	325
70	700	840	980	1000	365
FREEWAYS					
55	1000	1000	1000	1000	250
60	1000	1000	1000	1000	285
65	1000	1000	1000	1000	325
70	1000	1000	1000	1000	365

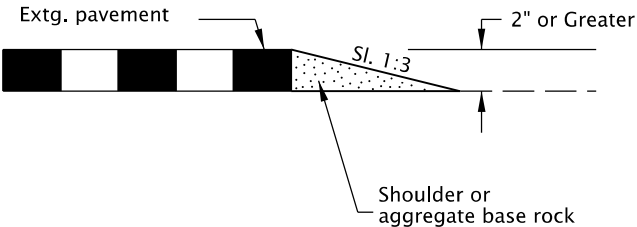
- NOTES:
- For Lane closures where W < 10', use "L" value for W = 10'.
  - For Shoulder closures where W < 10', use "L" value for W = 10' or calculate "L" using formula, for Speeds ≥ 45: L = WS, Speeds < 45: L = S<sup>2</sup>W/60, S = Speed, W=Width

TRAFFIC CONTROL DEVICES (TCD) SPACING TABLE				
★ SPEED (mph)	Sign Spacing (ft)			Max. Channelizing Device Spacing (ft)
	A	B	C	
20 – 30	100	100	100	20
35 – 40	350	350	350	20
45 – 55	500	500	500	40
60 – 70	700	700	700	40
Freeway	1000	1500	2640	40

- NOTES:
- Place traffic control devices on 10 ft. spacing for intersection and access radii.
  - When necessary, sign spacing may be adjusted to fit site conditions. Limit spacing adjustments to 30% of the "A" dimension for all speeds.

NOTES:

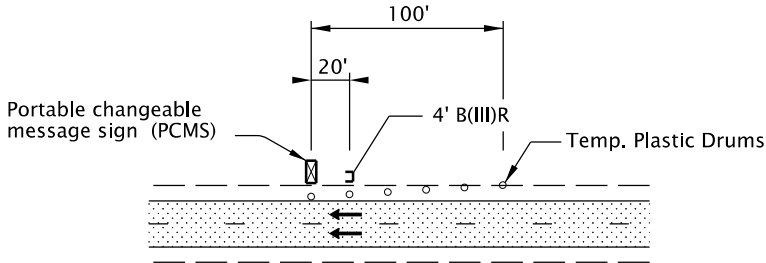
- When paved shoulders adjacent to excavations are less than four feet wide protect longitudinal abrupt edge as shown.
- Use aggregate wedge when abrupt edge is 2 inches or greater.



EXCAVATION ABRUPT EDGE

NOTES:

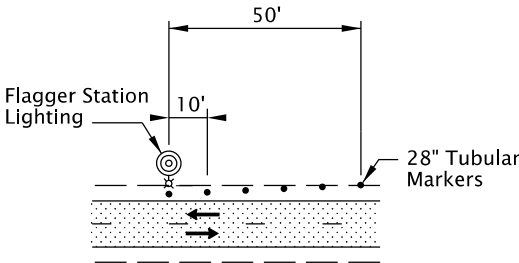
- Install PCMS beyond the outside shoulder, when possible.
- Use the appropriate type of barricade panels for PCMS location. Right shoulder, use Type B(III)R Left shoulder, use Type B(III)L
- Use six drums in shoulder taper on 20' spacing. The drums and barricade may be omitted when PCMS is placed behind a roadside barrier.
- Detail as shown is used for trailered and non-crashworthy components of:
  - Portable Traffic Signals
  - Smart Work Zone Systems



PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) INSTALLATION

NOTES:

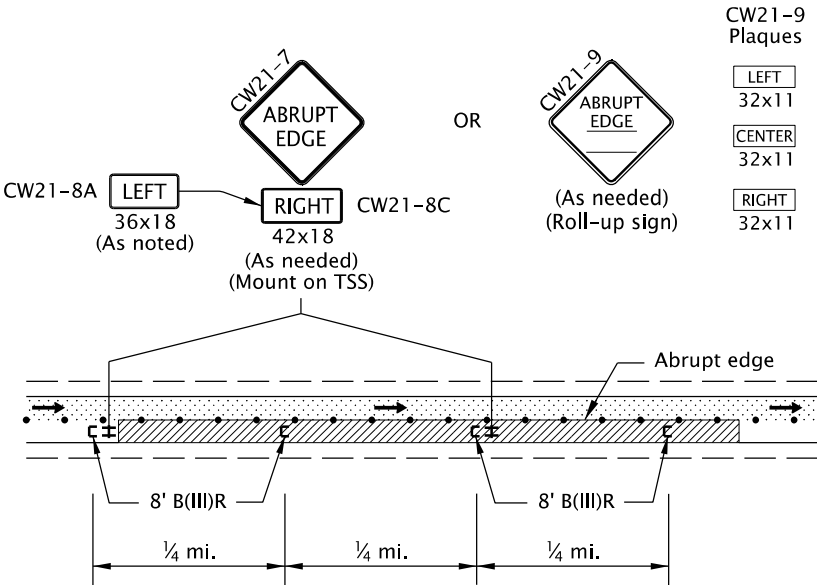
- Install Flagger Station Lighting beyond the outside shoulder, where practical.
- Use six tubular markers in shoulder taper on 10' spacing.
- Place cart / generator / power supply off of the shoulder, as far as practical.



FLAGGER STATION LIGHTING DELINEATION

NOTES:

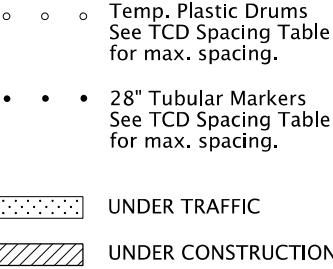
- Abrupt edges may be created by paving, operations, excavations or other roadway work. Use abrupt edge signing for longitudinal abrupt edges of 1 inch or greater.
- If the excavation is located on left side of traffic, replace the 8' B(III)R barricades with 8' B(III)L barricades and replace the "RIGHT" (CW21-8C) riders with "LEFT" (CW21-8A) riders.
- Continue signing and other traffic control devices throughout excavation area at spacings shown.
- If roll-up signs are used, attach the correct (CW21-9) plaques to the sign face using hook and loop fasteners. Place roll-up signs in advance of barricades.



TYPICAL ABRUPT EDGE DELINEATION

GENERAL NOTES FOR ALL TCP DRAWINGS:

- Signs and other Traffic Control Devices (TCD) shown are the minimum required.
- Place a barricade approx. 20' ahead of all sequential arrow boards.
- Arrows shown in roadway are directional arrows to indicate traffic movements.
- All signs are 48" x 48" unless otherwise shown. Use flourescent orange sheeting for the background of all temporary warning signs.
- All diamond shaped warning signs mounted on barrier sign supports shall be 36" by 36". All other signs mounted on barrier sign supports shall not exceed 12 sq. ft. in total sign area.
- Low speed highways have a pre-construction posted speed of 40 mph or less. High speed highways have a pre-construction posted speed of > 40 mph.
- Do not locate sign supports in locations designated for bicycle or pedestrian traffic.
- Combine drawing details to complete temporary traffic control for each work activity.
- To be accompanied by Drg. Nos. TM820 & TM821.



CALC. BOOK NO. TM09-01

BASELINE REPORT DATE 01-JAN-2019

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS  
TABLES, ABRUPT EDGE AND  
PCMS DETAILS

2018

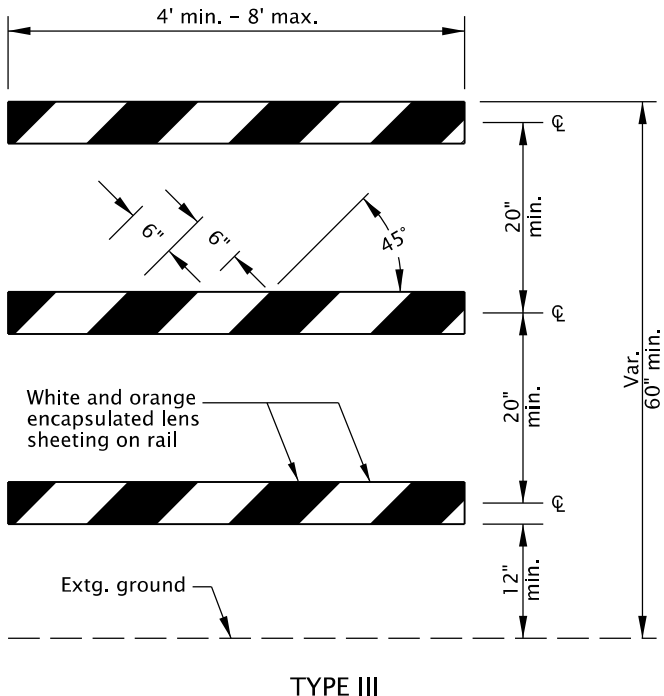
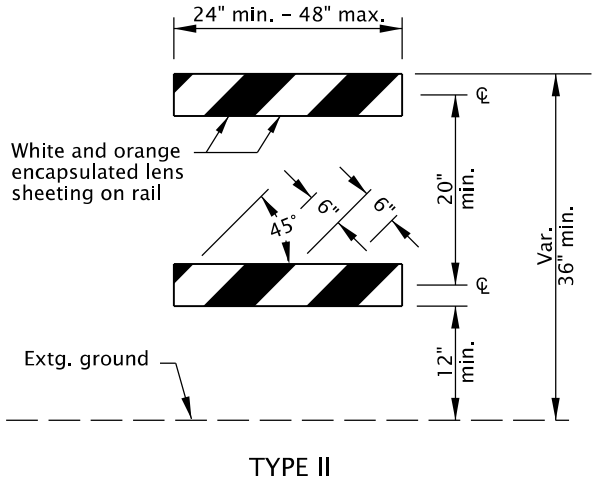
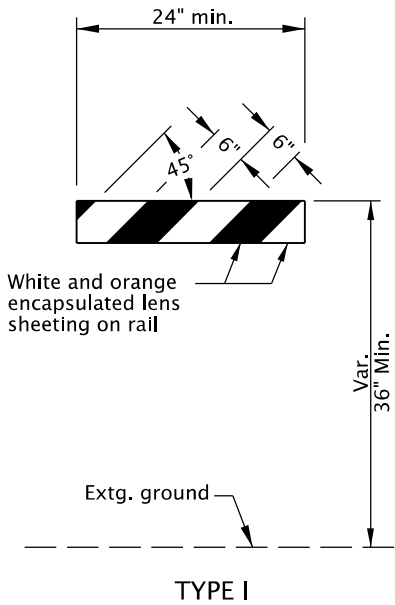
DATE	REVISION	DESCRIPTION

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tm820.dgn 01-JAN-2019

TM820



BARRICADE RAIL LAYOUT

- GENERAL NOTES FOR ALL DETAILS:
- Sandbags (approximately 25 lb sack filled with sand) may be placed on lower frame to provide additional ballast.
  - Ballast shall not extend above bottom rail or be suspended from barricade.
  - For rails less than 36" long, 4" wide stripes shall be used.
  - Rails must be 8" min. to 12" max. in height.
  - Use barricades from ODOT Qualified Products List (QPL).
  - Use 4' Type III barricades where horizontal space is limited.
  - Do not block bike lanes or shoulders unless the facility is properly closed and signed.
  - Do not place barricades in sidewalks unless sidewalk is closed and a temporary pedestrian accessible route (TPAR) is signed according to the TCP. See Dwg. No. TM 844.

- NOTES:
- Markings for barricade rails shall slope downward at an angle of 45° in the direction traffic is to pass.
  - Where a barricade extends entirely across a roadway, it is desirable that the stripes slope downward in the direction toward which traffic must turn in detouring.
  - Where both right and left turns are provided for, slope the chevron striping downward in both directions from the center of the barricade.
  - For full roadway closures, the C or LR barricade may be used. Extend barricades completely across roadway unless access is required for local road users.

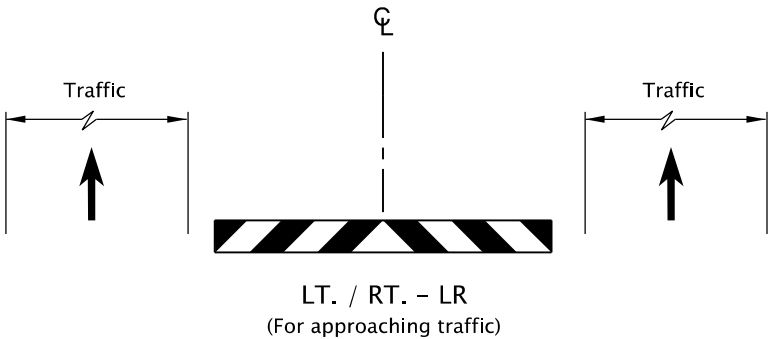
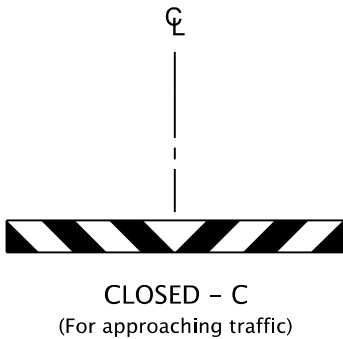
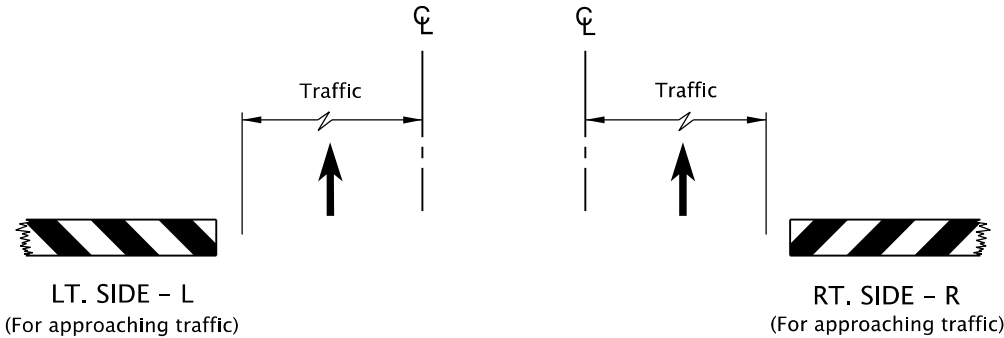
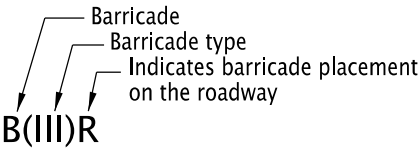


DIAGRAM FOR BARRICADE PLACEMENT AND SLOPE MARKING



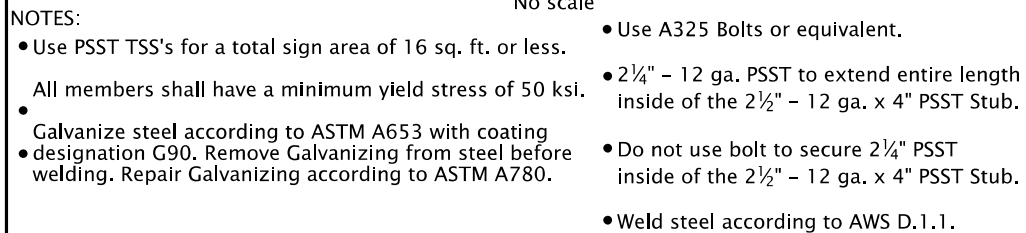
BARRICADE NOTATION

CALC. BOOK NO. N/A		BASELINE REPORT DATE 01-JAN-2019	
<i>The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.</i>		NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications	
		OREGON STANDARD DRAWINGS	
		TEMPORARY BARRICADES	
		2018	
		DATE	REVISION DESCRIPTION
		01-2019	REVISED NOTES

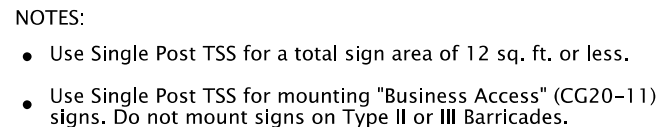




DOUBLE POST  
TEMPORARY SIGN SUPPORT (TSS)



PERFORATED STEEL SQUARE TUBE (PSST)  
TEMPORARY SIGN SUPPORT (TSS)



SINGLE POST  
TEMPORARY SIGN SUPPORT (TSS)



TEMPORARY SIGN SUPPORT GENERAL NOTES:

- DO NOT TIP OVER TSS AT ANY TIME.
- Do not locate TSS's in locations that block pedestrian/bicycle traffic.
- For wooden TSS's, use either Douglas Fir or Hem Fir, which is surfaced four sides (S4S) and free of heart center (FOHC).
- See "Temporary Sign Placement" detail on TM822 for sign installation heights.
- Do not place or stack ballast more than 24" above the ground.
- When sign is inconsistent with current work zone conditions, cover sign; or turn sign 90 degrees away from approaching traffic. Remove TSS from roadway when signing is not needed for more than 3 days.
- Place a minimum of 50 lbs of sandbags on each of the four TSS supports legs. (25 lb. max per bag) (min. 100 lbs per side of each TSS).

NOTES:

- Apply fluorescent orange, ANSI Type VIII or IX retroreflective sheeting to TSS posts, as shown, for all temporary signs, except "STOP" and "DO NOT ENTER". For "STOP" and "DO NOT ENTER" signs, used red ANSI Type III or IV retroreflective sheeting on the TSS posts.
- Apply sign post retroreflectivity to each TSS post facing front; and to the left and right sides of the TSS, as shown. Use 3" wide sheeting for wood post TSS's. Use 2" wide sheeting for PSST TSS's.
- Sheeting may be applied directly to post material; or applied to a rigid, lightweight substrate, then securely attached to the posts.

## SIGN POST REFLECTIVE SHEETING PLACEMENT

CALC. BOOK NO. \_\_\_\_\_ N/A

BASELINE REPORT DATE	01-JAN-2019
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NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

**OREGON STANDARD DRAWINGS**

## TEMPORARY SIGN SUPPORTS

2018

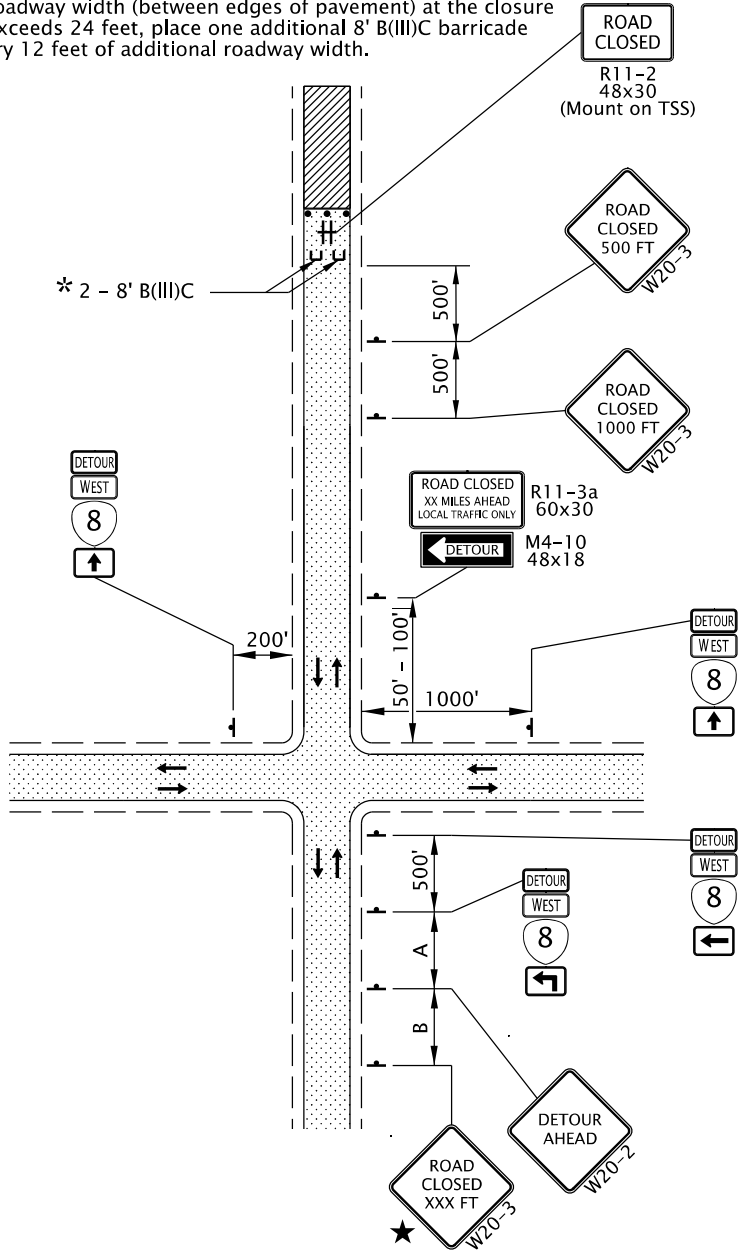
DATE	REVISION DESCRIPTION
01-2019	REVISED NOTES

*The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.*

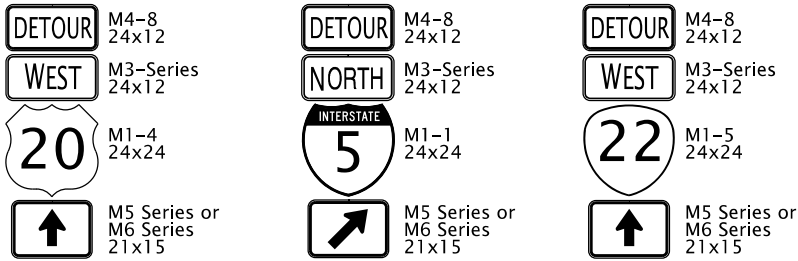


NOTES:  
If closure point is less than 1500 ft. from nearest intersection, use a "ROAD CLOSED TO THRU TRAFFIC" (R11-4) sign in place of the "ROAD CLOSED XX MILES AHEAD" sign.

\* If the roadway width (between edges of pavement) at the closure point exceeds 24 feet, place one additional 8' B(III)C barricade for every 12 feet of additional roadway width.

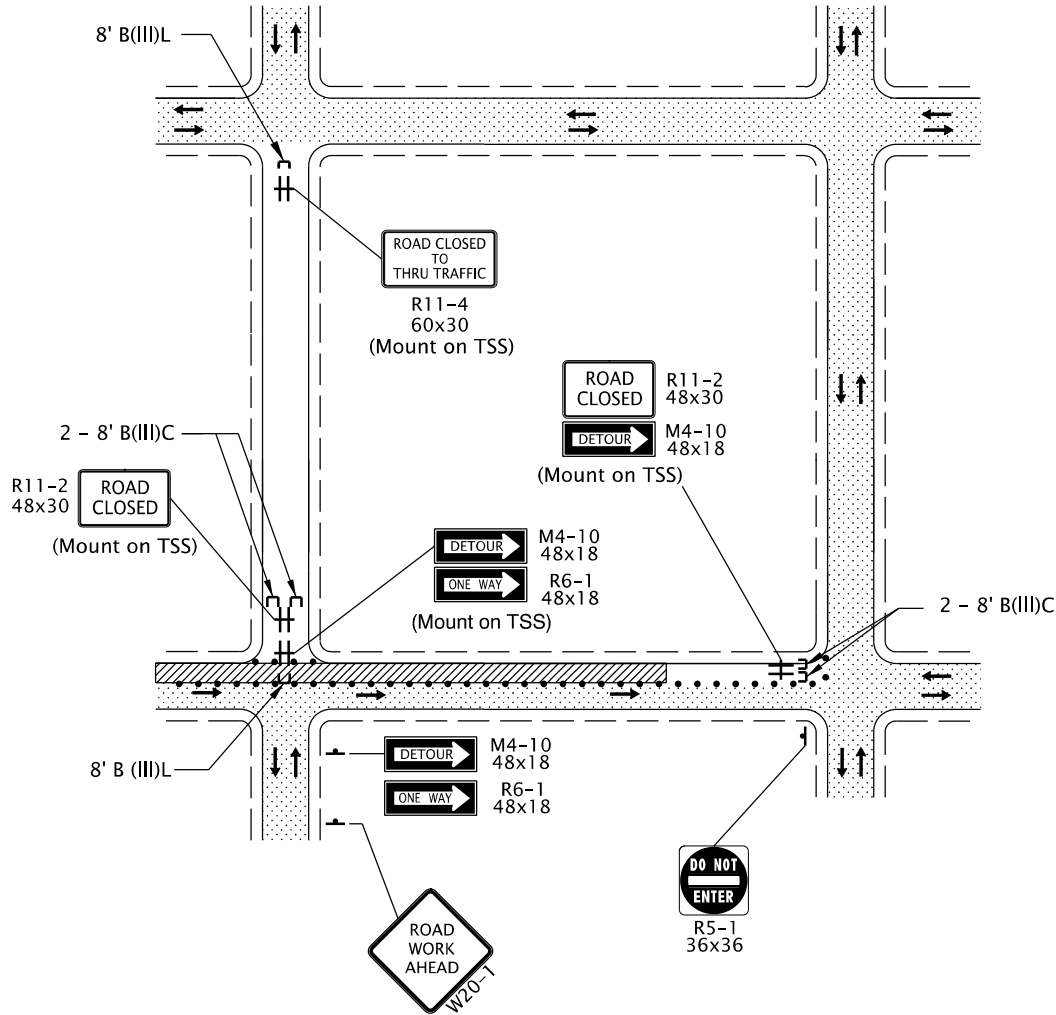


TYPICAL ROAD CLOSURE WITH DETOUR



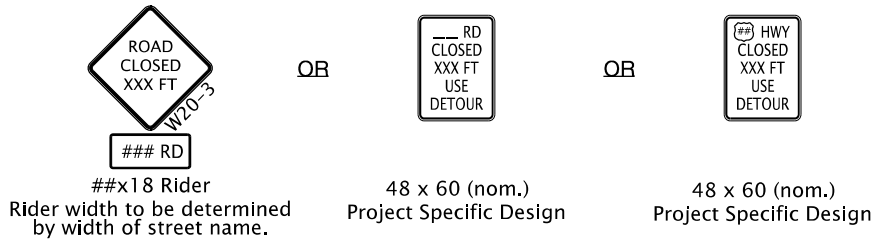
TYPICAL TRAILBLAZER ASSEMBLY

NOTE:  
When detour routes overlap, each Route Shield will include a separate cardinal direction, detour, and directional arrow auxiliary sign assembly.

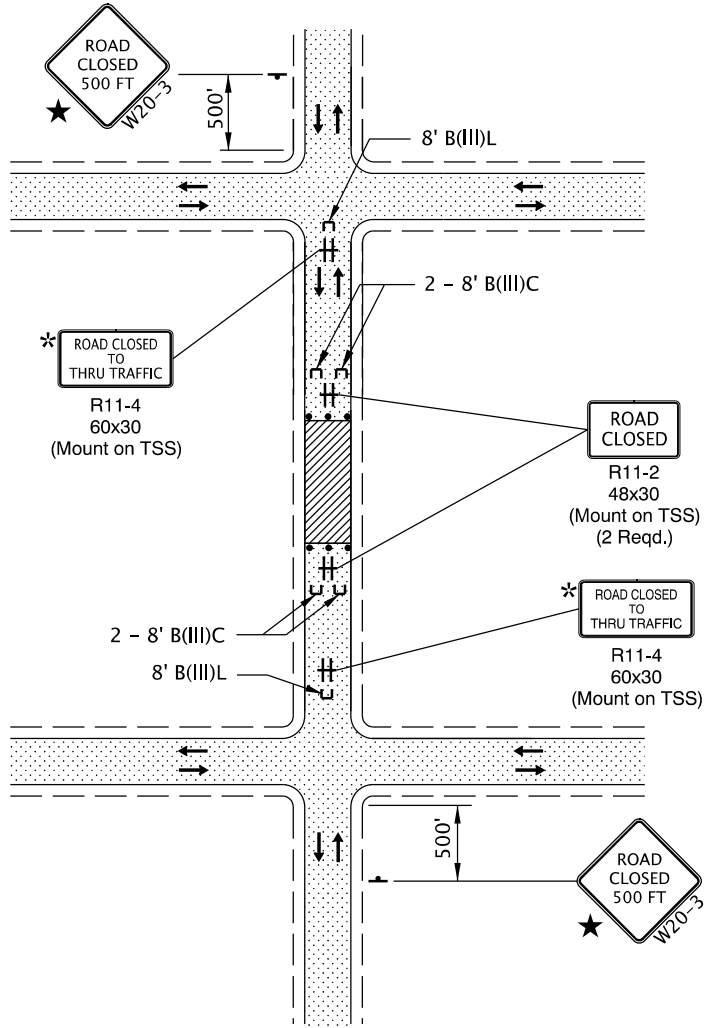
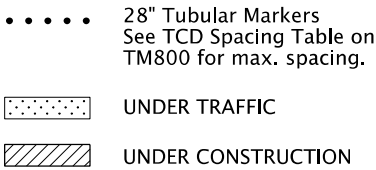


TYPICAL PARTIAL ROAD CLOSURE

GENERAL NOTES FOR ALL DETAILS:  
★ A "Street Name" rider may be used to enhance Road Closure signing; or provide a project specific design; or, as shown in the traffic control plan.



- Use a minimum of two Type III barricades for a road closure. For roads  $\geq 36'$  wide between curbs or edge of pavement, use a minimum of three Type III barricades for the closure point.
- For full road closures, the C or LR barricade may be used.
- Place additional signing as directed.
- To determine sign spacing A, B, & C, use the "TRAFFIC CONTROL DEVICES (TCD) SPACING TABLE" on Drg. TM800.
- To be accompanied by Drg. Nos. TM820 & TM821.



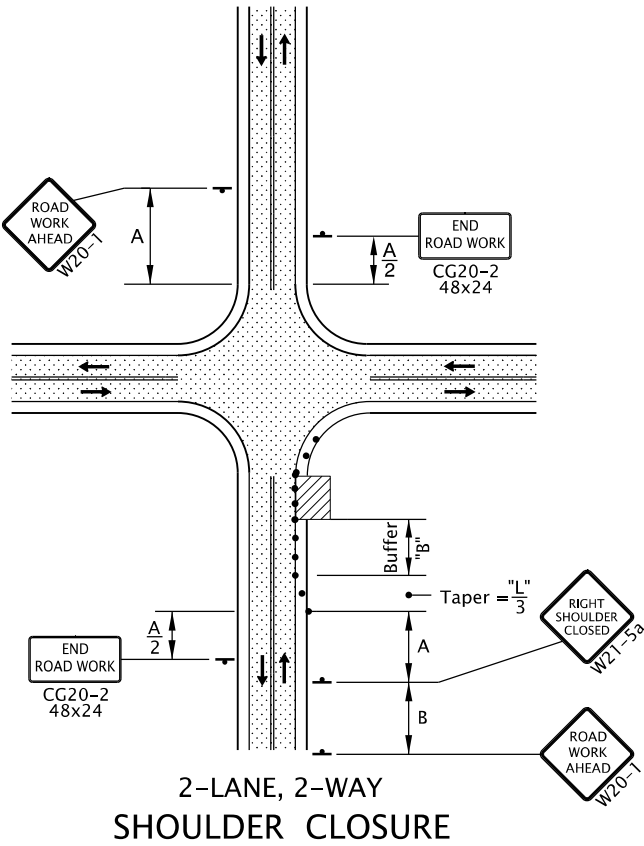
NOTE:  
\* If accesses exist between intersection and point of closure, install "ROAD CLOSED TO THRU TRAFFIC" sign as shown.

TYPICAL ROAD CLOSURE

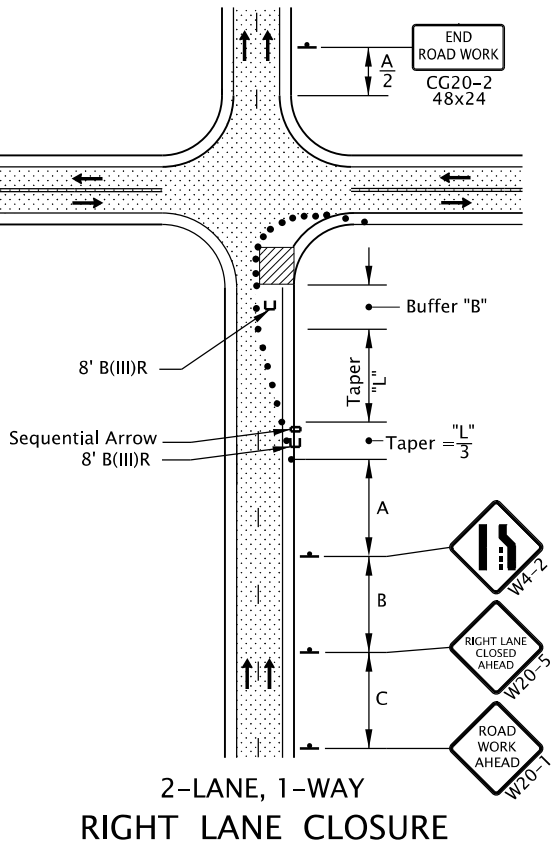
CALC. BOOK NO. N/A		BASELINE REPORT DATE 01-JAN-2019	
<i>The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.</i>		NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications	
		OREGON STANDARD DRAWINGS	
		CLOSURE DETAILS	
		2018	
		DATE	REVISION DESCRIPTION
		01-2018	REVISED DRAWING



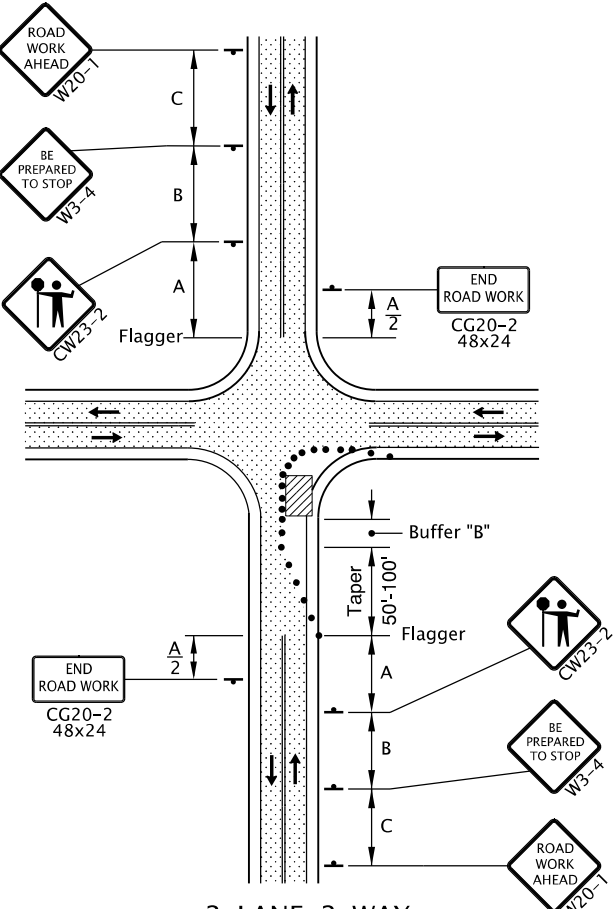
tm841.dgn 01-JAN-2019



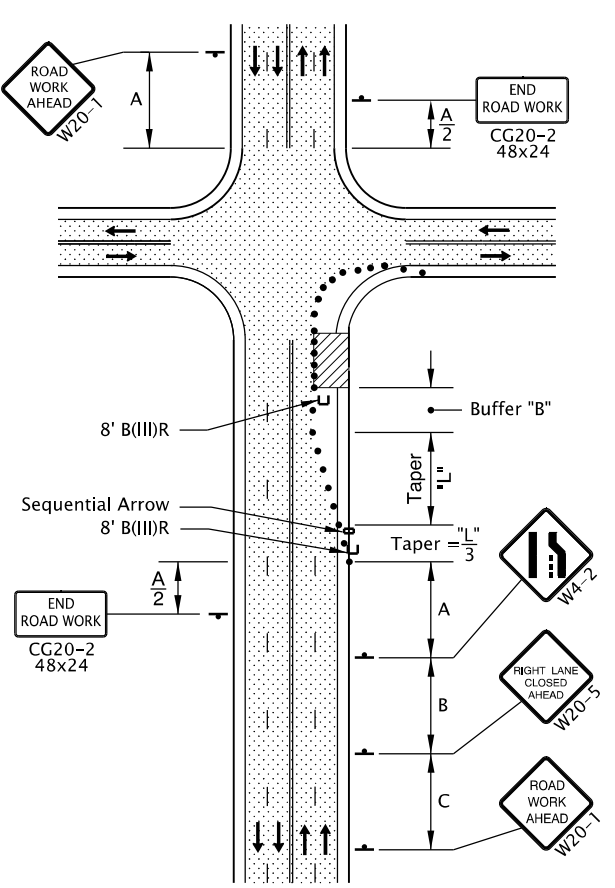
2-LANE, 2-WAY  
SHOULDER CLOSURE



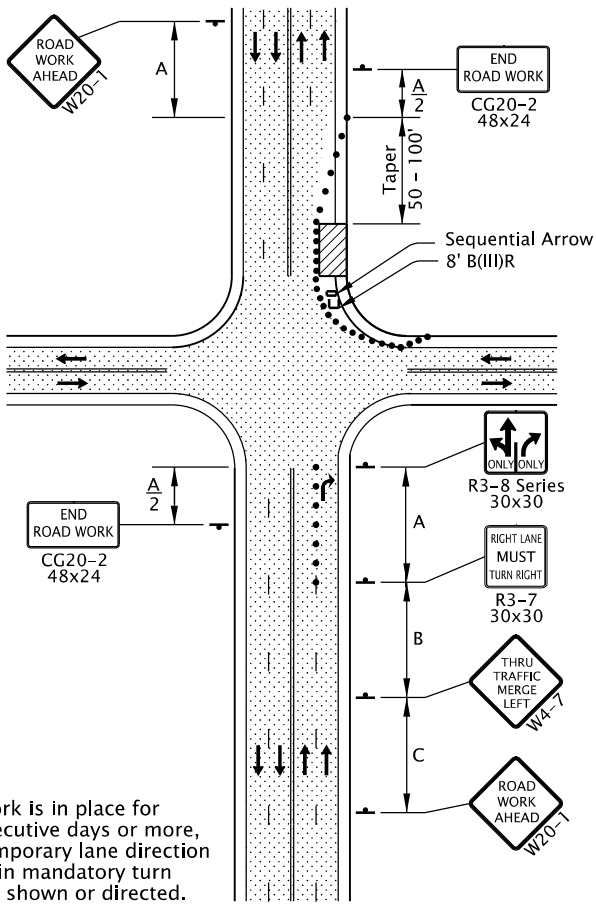
2-LANE, 1-WAY  
RIGHT LANE CLOSURE



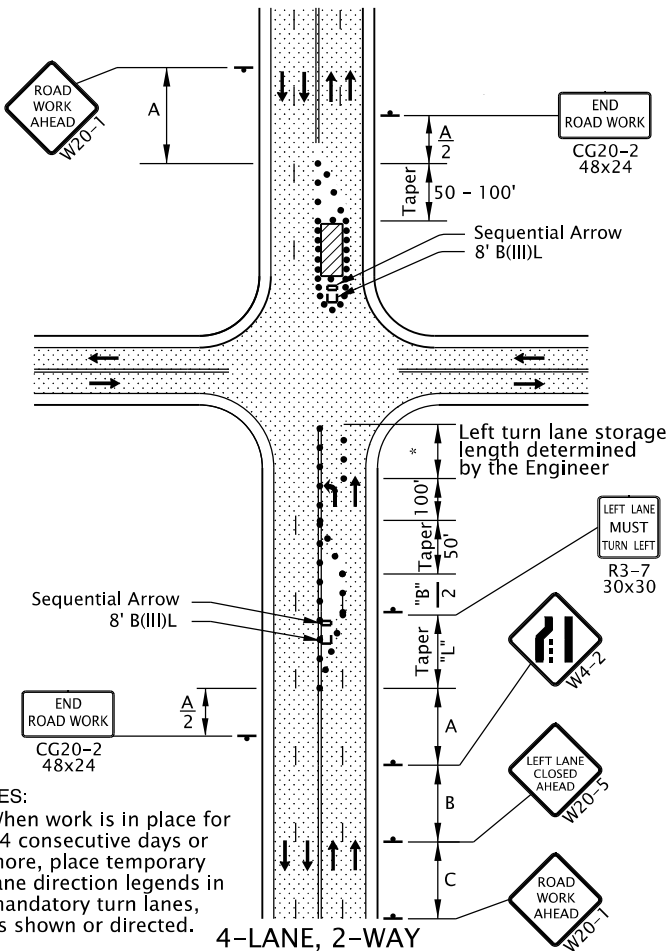
2-LANE, 2-WAY  
ONE LANE CLOSURE



4-LANE, 2-WAY  
RIGHT LANE CLOSURE, NEAR SIDE



4-LANE, 2-WAY  
RIGHT LANE CLOSURE, FAR SIDE



4-LANE, 2-WAY  
LEFT LANE CLOSURE, FAR SIDE

- NOTES:
- When work is in place for 14 consecutive days or more, place temporary lane direction legends in mandatory turn lanes, as shown or directed.

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- GENERAL NOTES FOR ALL DETAILS:
- Additional Traffic Control Measures (TCM) may be required for all legs of the intersection.
  - The "FLAGGER" (CW23-2) symbol sign shall be used only in conjunction with the "BE PREPARED TO STOP" (W3-4) sign.
  - To determine Taper Length ("L") and Buffer Length ("B"), use the "MINIMUM LENGTHS TABLE" on Drg. TM800.
  - For left lane or shoulder work, place TCD to close left lane or shoulder. Use "LEFT LANE CLOSED AHEAD" (W20-5) sign, "LEFT LANE ENDS" (W4-2L) symbol sign, or "LEFT SHOULDER CLOSED" (W21-5a) sign, where applicable.
  - To determine sign spacing A, B, and C, use "TRAFFIC CONTROL DEVICES (TCD) SPACING TABLE" on Drg. TM800.
  - When a through road intersects within the work zone, place a "ROAD WORK AHEAD" (W20-1) sign in advance of the intersection at sign spacing A.
  - Use plastic drums in lane closure tapers when the posted speed is 45 mph or greater.
  - Where shoulder width is limited, Sequential Arrow may be placed within the lane closure taper.
  - Place channelizing devices around intersection radii and construction areas at 10' spacing.
  - Install a "BICYCLES ON ROADWAY" (CW11-1) sign in advance of the closure when a bike lane is closed, or when the shoulder is closed and bikes are expected.
  - To be accompanied by Drg. Nos. TM820, TM821 & TM840.

- • • • • 28" Tubular Markers  
See TCD Spacing Table on TM800 for max. spacings.
- • • 28" Tubular Markers  
See TCD Spacing Table on TM800 for max. spacings.

UNDER TRAFFIC  
UNDER CONSTRUCTION

CALC. BOOK NO. _____ N/A _____	BASELINE REPORT DATE _____ 01-JAN-2019 _____
<i>The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.</i>	NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications
	<b>OREGON STANDARD DRAWINGS</b>
	<b>INTERSECTION WORK ZONE DETAILS</b>
	2018
	DATE REVISION DESCRIPTION



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ID: 5b2354ea-8f6e-4bd7-8bc0-83f37df63933

Steve Townsend

Steve.Townsen@portlandoregon.gov

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Envelope Summary Events	Status	Timestamps
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Browsers:	Final release versions of Internet Explorer® 6.0 or above (Windows only); Mozilla Firefox 2.0 or above (Windows and Mac); Safari™ 3.0 or above (Mac only)
PDF Reader:	Acrobat® or similar software may be required to view and print PDF files
Screen Resolution:	800 x 600 minimum



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