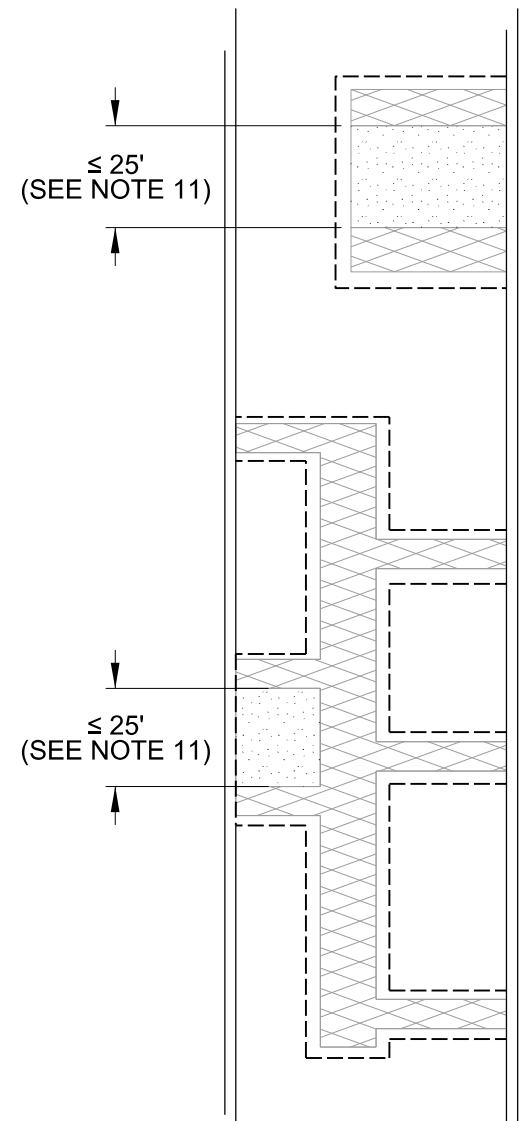
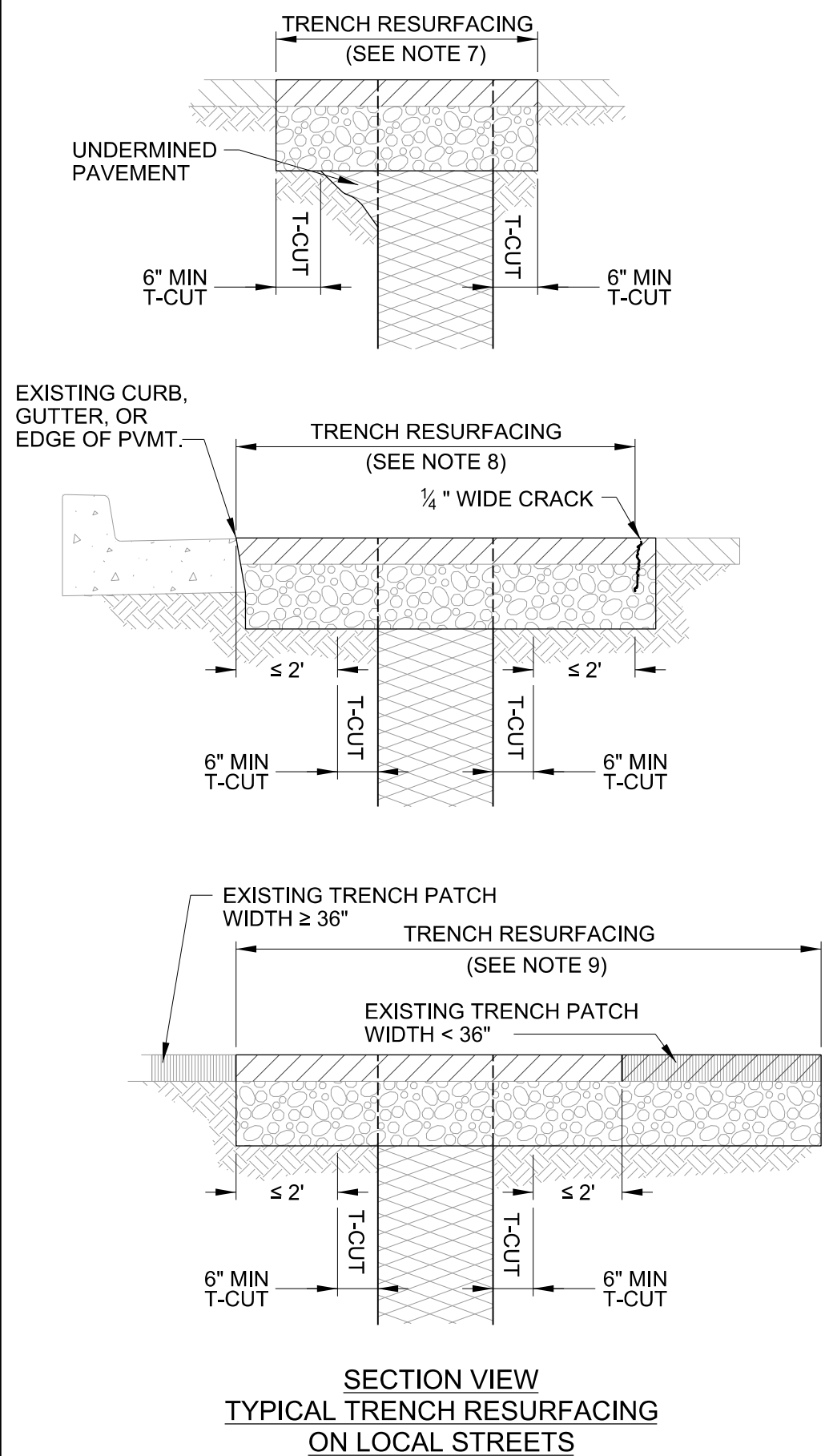
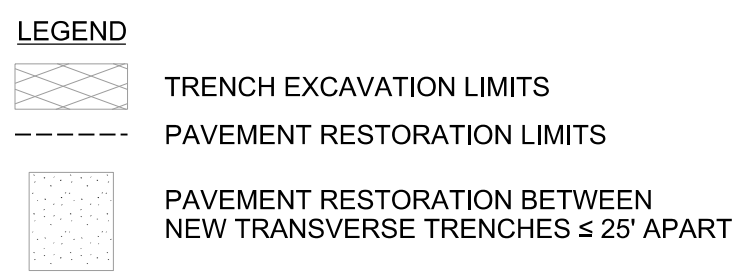


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**PLAN VIEW  
TYPICAL TRENCH RESURFACING  
ON LOCAL STREETS**



**NOTES:**

1. LOCAL STREETS ARE DEFINED AS RESIDENTIAL STREETS WITH NO LANE MARKINGS AND NOT A TRANSIT BUS ROUTE.
2. RESTORE PAVEMENT AND BASE WITH 3" ASPHALT ON 8" AGGREGATE BASE ROCK, OR MATCH EXISTING (WHICHEVER IS GREATER) UP TO MAX 6" ASPHALT ON 8" AGGREGATE.
3. MAX DEPTH OF 4" THICK ASPHALT LIFTS. ALLOW EACH LIFT TO COOL TO 160°F BEFORE PLACING NEXT LIFT.
4. PROTECT NEW PAVEMENT FROM TRAFFIC UNTIL FINAL LIFT IS PLACED AND ASPHALT IS 160°F, OR COOLER.
5. NEW PAVEMENT SHALL BE LEVEL 2, PG 64-22 ASPHALT CONCRETE.
6. TRENCH RESURFACING WIDTH SHALL BE 3-FOOT MINIMUM. T-CUT ONLY AFTER TRENCH IS FULLY BACKFILLED AND COMPACTED.
7. SAWCUT A MIN. 6" OVERLAP T-CUT, MEASURED FROM EDGE OF TRENCH, AND DOWN THROUGH THE BASE MATERIAL. EXTEND T-CUT A MIN 6" BEYOND EDGE OF ANY DAMAGED OR UNDERMINED PAVEMENT.
8. IF NEW T-CUT SAWCUT EDGE IS LESS THAN 2 FEET FROM EXISTING ¼" WIDE CRACK, CURB, GUTTER OR EDGE OF STREET PAVEMENT - REPLACE THE PAVEMENT IN BETWEEN.
9. IF NEW T-CUT SAWCUT EDGE IS LESS THAN 2 FEET FROM EXISTING PATCH:
  - AND EXISTING TRENCH PATCH IS ≥ 36" WIDE, REPLACE THE PAVEMENT IN BETWEEN.
  - AND EXISTING TRENCH PATCH IS < 36" WIDE, REPLACE TO OPPOSITE SIDE OF EXISTING TRENCH.
10. SAWCUTS SHALL BE UNIFORM AND SMOOTH AND EITHER PARALLEL OR PERPENDICULAR TO THE ROADWAY CENTERLINE. ALIGN SAWCUTS TO REMOVE ANY JAGGED, BROKEN OR IRREGULAR EDGES.
11. FULLY RESTORE PAVEMENT BETWEEN ALL NEW TRANSVERSE TRENCHES THAT EXTEND TO EDGE OF PAVEMENT, CURB, OR GUTTER AND ARE ALSO ≤ 25 FT APART.
12. TRENCH BACKFILL SHALL MEET COMPACTION AND DENSITY REQUIREMENTS. PROVIDE TESTING RESULTS UPON REQUEST BY THE INSPECTOR.
13. PBOT MAY APPROVE GRIND + INLAY IN LIEU OF T-CUT WHEN APPROPRIATE. ANY SUCH APPROVAL SHALL BE OBTAINED IN WRITING THROUGH THE STREET OPENING PERMIT PROCESS.
14. SAND SEAL ALL JOINTS.

<p>The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user.</p>	<p><b>PBOT PORTLAND BUREAU OF TRANSPORTATION</b></p> <p>City Engineer</p>		
	<p>Standard Drawing Title</p> <p><b>AC PAVEMENT TRENCH RESURFACING ON LOCAL STREETS</b></p>		
<p>Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications.</p>	Effective Date:	02-26-2018	Standard Drawing No.
	Calc. Book No.:	505	
	Std. Drg. Report Date:	-----	